

**NATIONAL REGISTER OF HISTORIC PLACES
INVENTORY - NOMINATION FORM**

(Type all entries - complete applicable sections)

STATE:
California

COUNTY:
Los Angeles

FOR NPS USE ONLY

ENTRY NUMBER	DATE
	JUL 16 1973



1. NAME

COMMON:
Oaklawn Bridge AND WAITING STATION

AND/OR HISTORIC:

2. LOCATION

STREET AND NUMBER:
Between Oaklawn and Fair Oaks Avenues

CITY OR TOWN:
South Pasadena

STATE
California

CODE 06 COUNTY: Los Angeles CODE 037

3. CLASSIFICATION

CATEGORY (Check One)	OWNERSHIP	STATUS	ACCESSIBLE TO THE PUBLIC
<input type="checkbox"/> District <input checked="" type="checkbox"/> Building <input type="checkbox"/> Site <input checked="" type="checkbox"/> Structure <input type="checkbox"/> Object	<input checked="" type="checkbox"/> Public <input type="checkbox"/> Private <input type="checkbox"/> Both	<input checked="" type="checkbox"/> Occupied <input type="checkbox"/> Unoccupied <input type="checkbox"/> Preservation work in progress	Yes: <input type="checkbox"/> Restricted <input checked="" type="checkbox"/> Unrestricted <input type="checkbox"/> No

PRESENT USE (Check One or More as Appropriate)

<input type="checkbox"/> Agricultural <input type="checkbox"/> Commercial <input type="checkbox"/> Educational <input type="checkbox"/> Entertainment	<input type="checkbox"/> Government <input type="checkbox"/> Industrial <input type="checkbox"/> Military <input type="checkbox"/> Museum	<input type="checkbox"/> Park <input type="checkbox"/> Private Residence <input type="checkbox"/> Religious <input type="checkbox"/> Scientific	<input checked="" type="checkbox"/> Transportation <input type="checkbox"/> Other (Specify)	<input type="checkbox"/> Comments
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4. OWNER OF PROPERTY

OWNER'S NAME:
City of South Pasadena

STREET AND NUMBER:
1424 Mission Street

CITY OR TOWN:
South Pasadena

STATE:
California

CODE
06

5. LOCATION OF LEGAL DESCRIPTION

COURTHOUSE, REGISTRY OF DEEDS, ETC:
Los Angeles Hall of Records

STREET AND NUMBER:
320 West Temple Street

CITY OR TOWN:
Los Angeles

STATE:
California

CODE
06

6. REPRESENTATION IN EXISTING SURVEYS

TITLE OF SURVEY:
Architects Design

DATE OF SURVEY: 1906 ☐ Federal ☐ State ☐ County ☒ Local

DEPOSITORY FOR SURVEY RECORDS:
Greene & Greene Library

STREET AND NUMBER:
The Gamble House #4 - Westmorland Place

CITY OR TOWN:
Pasadena

STATE:
California

CODE
06

SEE INSTRUCTIONS

STATE: California

COUNTY: Los Angeles

ENTRY NUMBER: JUL 16 1973

DATE:

FOR NPS USE ONLY

7. DESCRIPTION

CONDITION	(Check One)					
	<input type="checkbox"/> Excellent	<input checked="" type="checkbox"/> Good	<input type="checkbox"/> Fair	<input type="checkbox"/> Deteriorated	<input type="checkbox"/> Ruins	<input type="checkbox"/> Unexposed
	(Check One)			(Check One)		
	<input type="checkbox"/> Altered	<input type="checkbox"/> Unaltered	<input type="checkbox"/> Moved	<input checked="" type="checkbox"/> Original Site		

DESCRIBE THE PRESENT AND ORIGINAL (if known) PHYSICAL APPEARANCE

This bridge is a graceful, arched structure rising from an embankment at the west end curving to street level at the east, where a waiting station, constructed of boulders from the nearby Arroyo Seco, with a heavy wood-beamed roof of *Ludowici clay tile, terminates at the south balustrade. A detail of red brick is incorporated with the boulders, a combination typical of the architects' work. Presently the bridge is covered with ivy, which is periodically trimmed.

The bridge has six spans and a total length of 340 feet. The concrete arch is only twelve inches thick at the center. Reinforcing is twisted square bars of metal 7/16 of an inch in size and laid from 3'0" to 6 inches apart. Longitudinal bars 1½ inches in thickness are used in other sections. The coping is 8 inches at the base and 4 inches at the top. Forty-five tons of steel and 20,000 cubic feet of concrete mark it as a pioneer of its day.

Original sketch and plans for the bridge show five spans. This plan of construction was not acceptable to the Santa Fe Railway Co., and consequently, an additional pillar was added to the center of span #2, to satisfy the railway. Architects, Greene and Greene felt this was not necessary. They added the center pillar reluctantly because they felt it interfered with the grace of the span design and was not necessary for structural soundness. There are no lighting fixtures on the bridge altho they appear on the original plans.

In 1934, a United States Geodetic seal was implanted at the base of the tall concrete pylon at the northeast corner of the bridge.

*Imprinted on the

Ludowici Tile: T 12
Pat. July-19-98
Pat. Mar. 17-03



SEE INSTRUCTIONS

8. SIGNIFICANCE

PERIOD (Check One or More as Appropriate)

☐ Pre-Columbian☐ 16th Century☐ 18th Century☒ 20th Century☐ 15th Century☐ 17th Century☐ 19th Century

SPECIFIC DATE(S) (If Applicable and Known) 1906

AREAS OF SIGNIFICANCE (Check One or More as Appropriate)

Aboriginal

☐ Prehistoric☒ Historic☐ Agriculture☒ Architecture☐ Art☐ Commerce☐ Communications☐ Conservation☐ Education☒ Engineering☐ Industry☐ Invention☒ Landscape

Architecture

☐ Literature☐ Military☐ Music☐ Political☐ Religion/Phi-

losophy

☐ Science☐ Sculpture☐ Social/Human-

itarian

☐ Theater☒ Transportation☒ Urban Planning☐ Other (Specify)

STATEMENT OF SIGNIFICANCE

The foresight of the South Pasadena Realty and Improvement Co. in sponsoring a structure, such as the Oaklawn Bridge, shortly after the turn of the century is significant for the following reasons:

Historically -- Worthy of special note because it is the only bridge designed by Greene and Greene, internationally known architects.

Architecturally -- It is entirely free of the European styles in vogue in this part of the world, in the early 1900's, and it reflects the California style as conceived and brought to a flowering by these architects. The builder was Carl Leonardt.

Engineering -- Michael de Palo, an Italian, who pioneered in reinforced concrete, was the consulting engineer. The construction and design of concrete and metal mark it as an engineering achievement of its day.

Landscaping -- The fitting of the bridge and waiting station into its environment with the landscaping as planned by the architects is still of vital significance in the community.

Transportation -- This bridge for vehicle and foot traffic, serves a vital link between Oaklawn Avenue and Fair Oaks Avenue, spanning the Santa Fe Railway. The waiting station was intended for shelter for the electric car passengers.

Urban Planning -- It continues, after nearly 66 years, to be as effective and useful as the day it was built.



9. MAJOR BIBLIOGRAPHICAL REFERENCES

There are no major published works containing facts about this bridge and structure. A publication listed below contains a chapter on Charles Sumner and Henry Mather Greene.

McCoy, Ester - Five California Architects. Reinhold Publishing Corp. 1960.

Chapter on Greene & Greene - by Randell L. Makinson, Curator - Gamble House #4 Westmoreland Place, Pasadena, California.

10. GEOGRAPHICAL DATA

LATITUDE AND LONGITUDE COORDINATES DEFINING A RECTANGLE LOCATING THE PROPERTY				OR	LATITUDE AND LONGITUDE COORDINATES DEFINING THE CENTER POINT OF A PROPERTY OF LESS THAN TEN ACRES			
CORNER	LATITUDE				LONGITUDE			
	Degrees	Minutes	Seconds	Degrees	Minutes	Seconds		
NW	0	'	"	0	'	"		
NE	0	'	"	0	'	"		
SE	0	'	"	0	'	"		
SW	0	'	"	0	'	"		

APPROXIMATE ACREAGE OF NOMINATED PROPERTY: 2+

LIST ALL STATES AND COUNTIES FOR PROPERTIES OVERLAPPING STATE OR COUNTY BOUNDARIES

STATE:	CODE	COUNTY	CODE
STATE:	CODE	COUNTY:	CODE
STATE:	CODE	COUNTY:	CODE
STATE:	CODE	COUNTY:	CODE

11. FORM PREPARED BY

NAME AND TITLE:	
Margaret Leslie Fay, A.I.D.	
ORGANIZATION	DATE
Vice-Chairman & Historical Chairman South Pasadena Cultural Heritage Commission	April 11, 1972
STREET AND NUMBER:	
1424 Mission Street	
CITY OR TOWN:	STATE
South Pasadena	California 91030
	CODE
	06

12. STATE LIAISON OFFICER CERTIFICATION

NATIONAL REGISTER VERIFICATION

As the designated State Liaison Officer for the National Historic Preservation Act of 1966 (Public Law 89-665), I hereby nominate this property for inclusion in the National Register and certify that it has been evaluated according to the criteria and procedures set forth by the National Park Service. The recommended level of significance of this nomination is:

National ☐ State ☐ Local ☒

Name

Title State Liaison Officer

Date June 29, 1972

I hereby certify that this property is included in the National Register.

Chief, Office of Archeology and Historic Preservation

Date

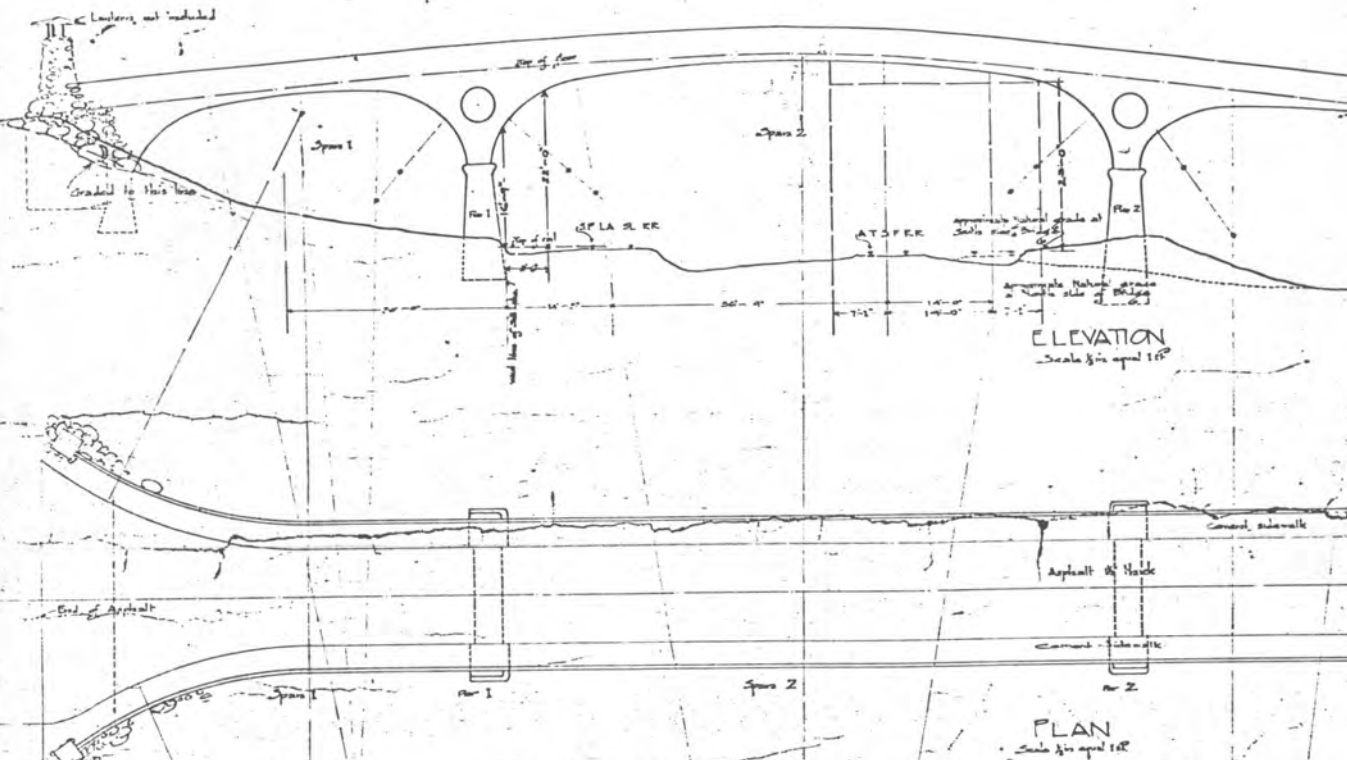
ATTEST:

Keeper of The National Register

Date

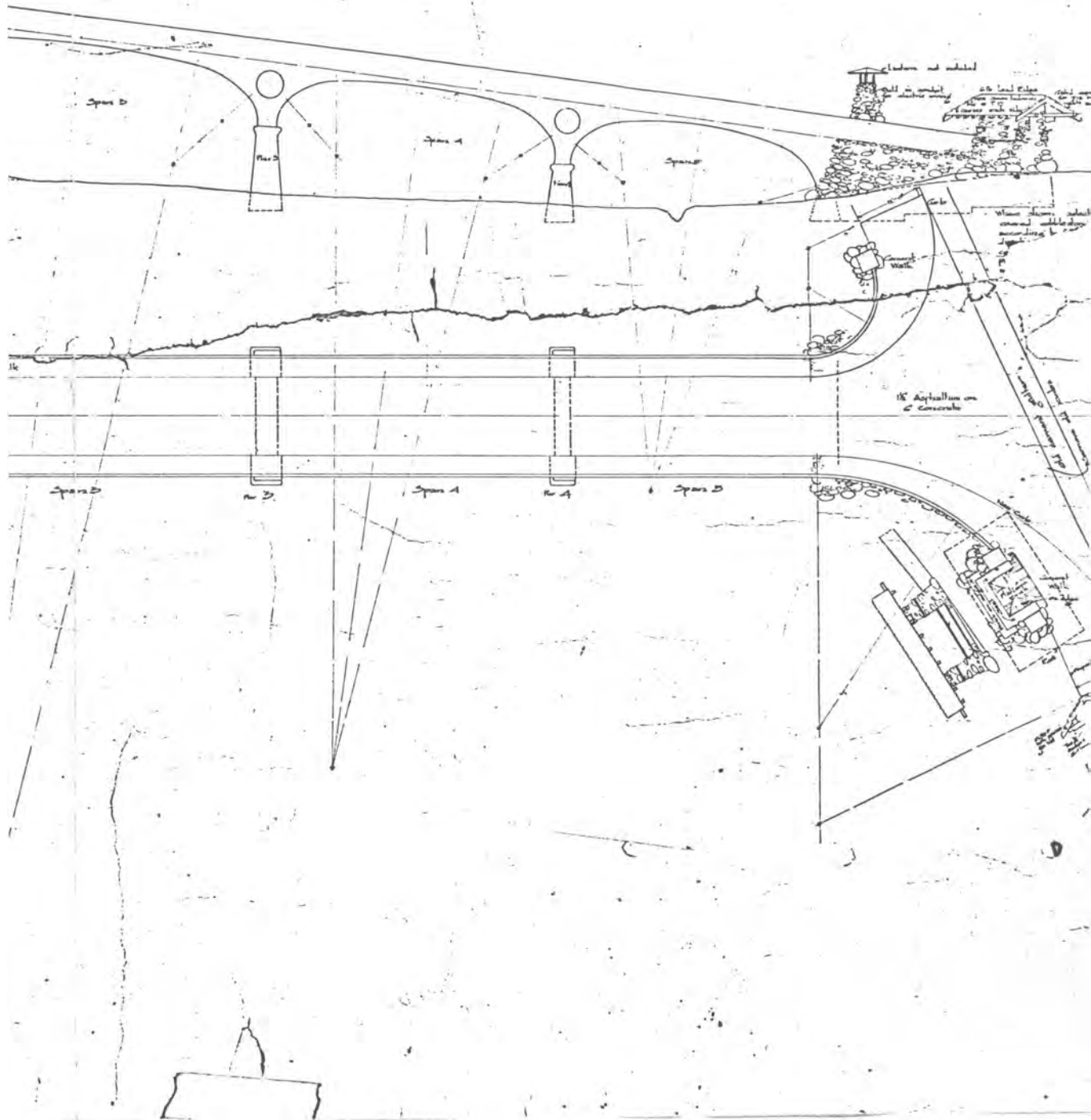
SEE INSTRUCTIONS

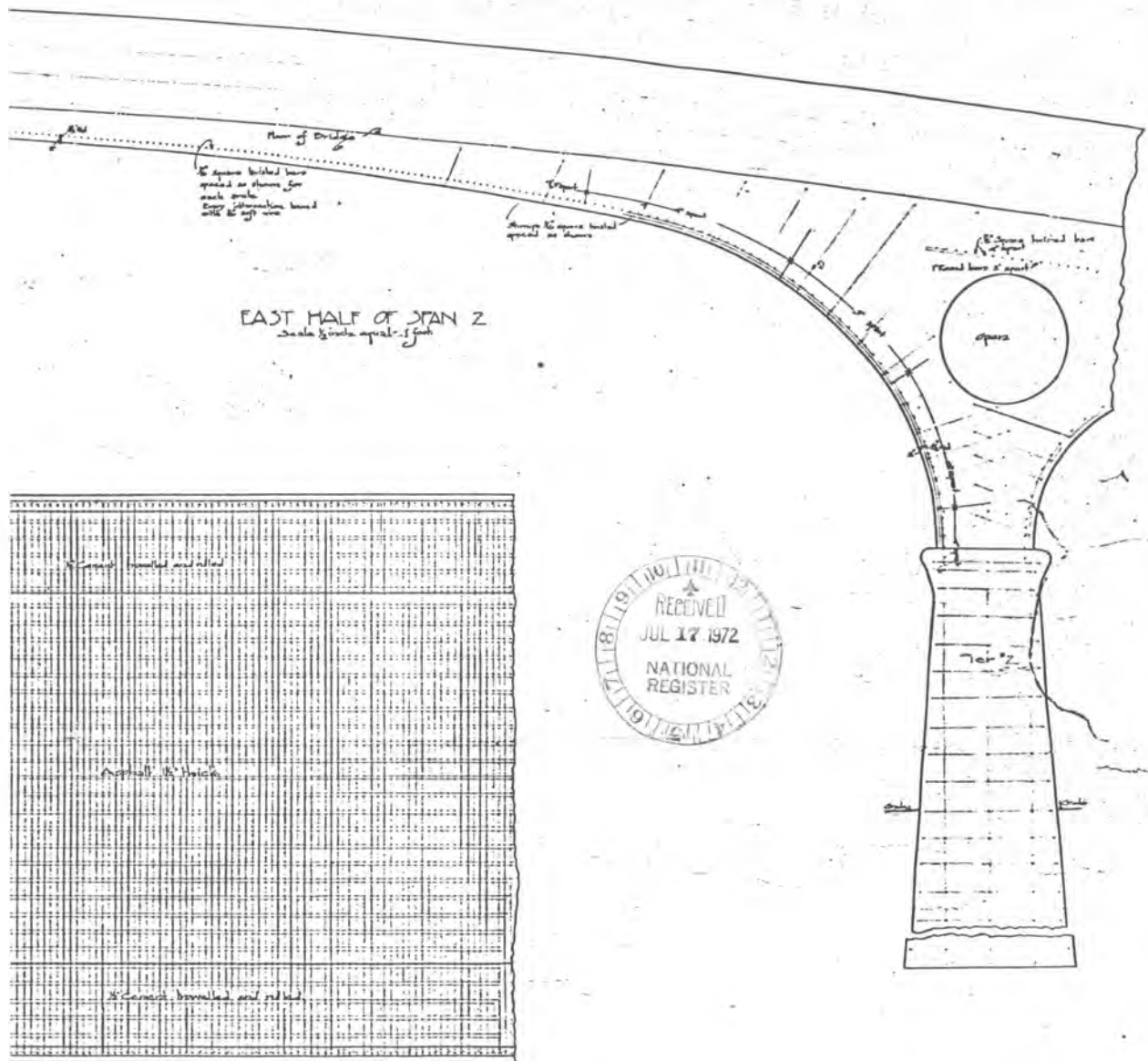
REINFORCED CONCRETE BRIDGE AT OAKLAWN
 FOR
 SOUTH PASADENA REALTY AND IMPROV
 GREENE AND GREENE ARCHTS. 722 GRANT BUILDING
 M. DE PALO, CONSULTING ENGINEER
 SHEET NO 1



IN 30 PASADENA CALIF.
REVEMENT COMPANY
BUILDING LOS ANGELES CALIF.
ENGINEER.

GREENE & GREENE LIBRARY
THE CAMPBELL PICTURES





PART PLAN EAST HALF of SPAN 2
Scale 1/2" = 1 foot

DESIGNED CONCRETE BRIDGE AT OAKLAWN, 30. PASADENA, CALIF.
SOUTH PASADENA REALTY AND IMPROVEMENT COMPANY.
AND GREENE ARCHTS, 722 GRANT BUILDING, 103 ANGELES CALIF.
M. DE PALO CONSULTING ENGINEER.

SHEET No 2.

EAST END - PER.

SIDE

DETAILS OF PERS AT WEST END OF BRIDGE

SCALE ~~ONE~~ INCH EQUALS ONE FOOT.
HALF

SO. PASADENA REALTY & IMPROVEMENT CO., SO. PASADENA.

No. 4.

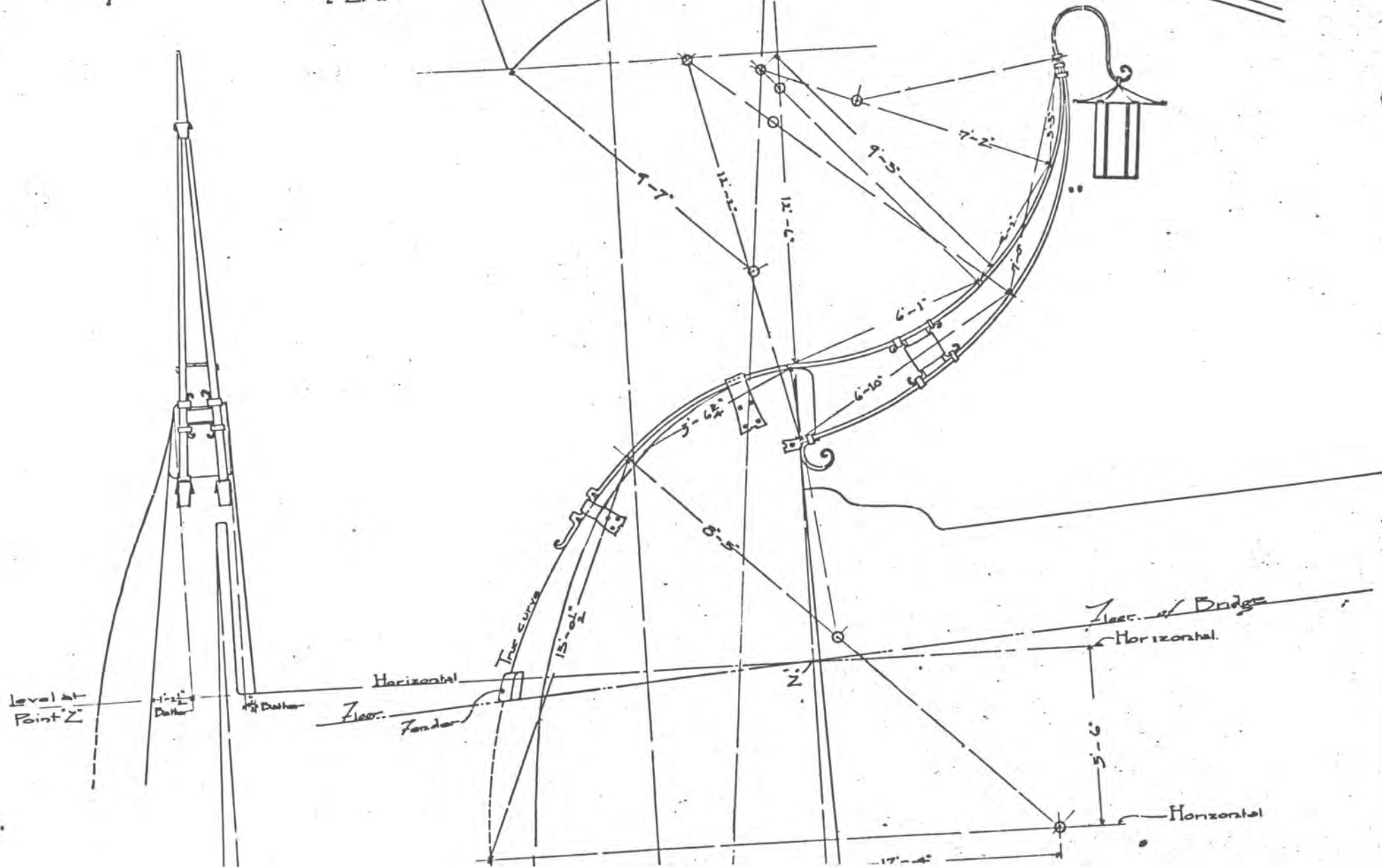
Center of Curves at
West End of Bridge.



GREENE & GREENE LIBRARY
THE GAMBLE HOUSE

A Tangent to curves (on plan)
at West end of bridge

PLAN

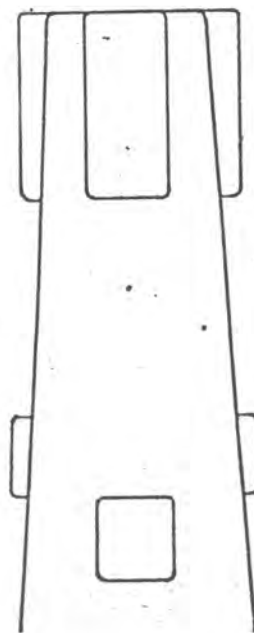
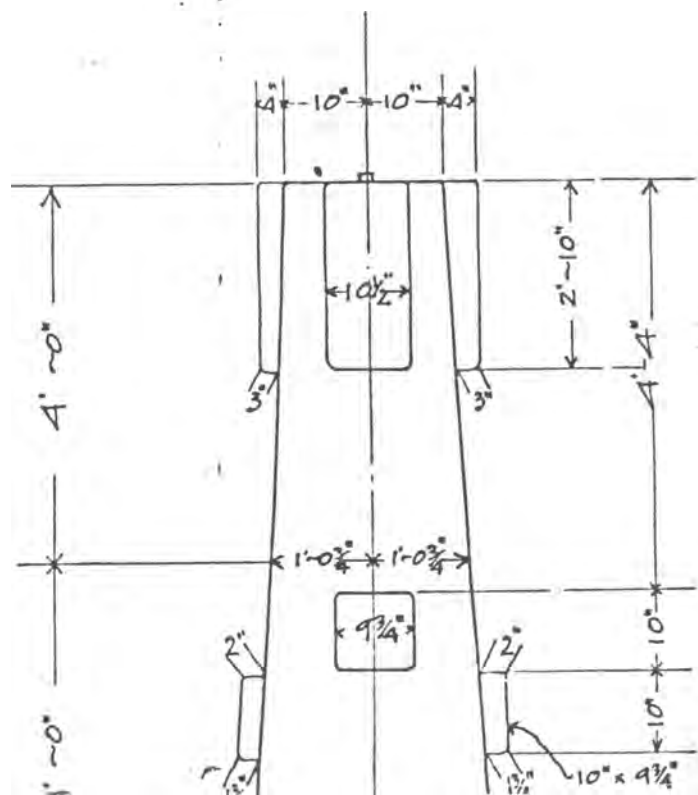


GREENE & GREENE LIBRARY
THE GAMBLE HOUSE

DETAIL OF PIERS AT EAST END OF BRIDGE.

Scale $\frac{1}{2}$ in. equals one foot.

SO. PASADENA REALTY & IMPROVEMENT CO. SO. PASADENA
SHEET NO. 5
JUNE 23rd '06

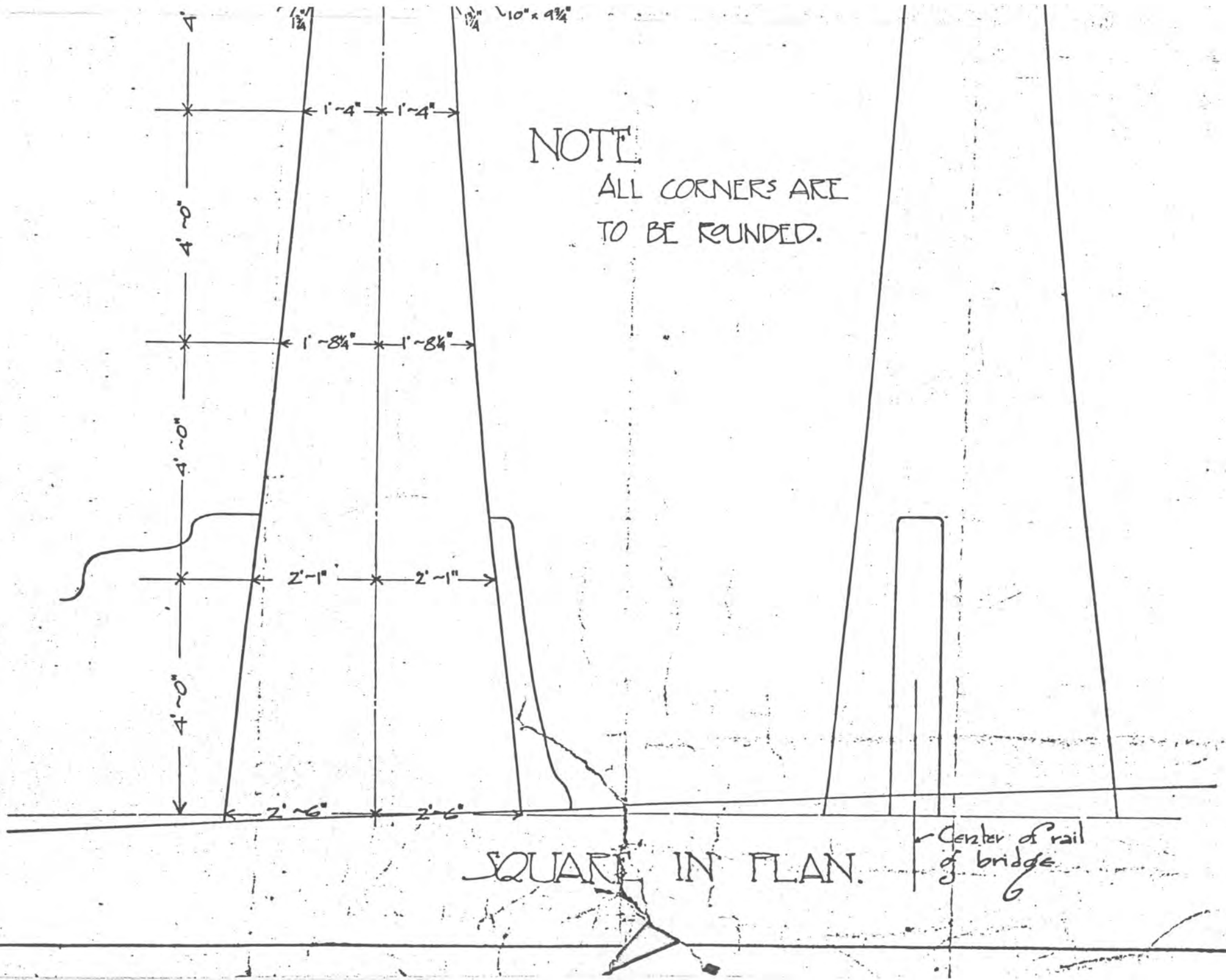


NOTE

ALL CORNERS ARE
TO BE ROUNDED.

SQUARE IN PLAN.

Center of rail
of bridge



United States Department of the Interior
National Park Service

National Register of Historic Places Continuation Sheet

Los Angeles County, California

Section number 7 Page 1 Name of Property Oaklawn Bridge and Waiting Station

The Oaklawn Bridge is a 340 ft. long by 20 ft. wide, six-span, reinforced concrete bridge connecting the Oaklawn subdivision to Fair Oaks Avenue, spanning the tracks of the Santa Fe Railway. Pasadena Architects Henry Mather Green and Charles Sumner Green with Italian Engineer Michael de Palo as consultant designed it in 1905 for pedestrian and horse-and-buggy traffic, originally consisting of five arches of variable span and rise. The arches of the asymmetric bridge vary in curvature and span as the piers height accommodates the slope of the land and provide maximum necessary clearance for railroad cars. The bridge rises from an embankment at the west, Oaklawn subdivision end, curving to street level at the east, Fair Oaks Avenue, where the south parapet abuts a Waiting Station, constructed of boulders from the nearby Arroyo Seco incorporated with red brick and a heavy wood-beamed roof of clay tile, a combination typical of the architects' work.

The primary decoration on the austere bridge structure is the chamfered caps on the impost blocks from which the arches spring, a small-diameter circular thru-hole at each pier above the impost block, and exposed form board patterning on the concrete surfaces. The concrete arch is only twelve inches thick at the center. Primary reinforcement consisted of twisted square rods placed longitudinally two inches from the intrados extending downward about a foot below the top of the coping in each pier, and vertical and horizontal rods each in pier. There was no continuous reinforcement along the roadbed.

The approximately 12 foot by 20 foot Waiting Station structure sits on a concrete slab topped with brick with two opposing concrete seats set in to the river rock walls. Originally the seat was also along the back. Stone and brick support the heavy timber roof structure whose beams extend beyond the clay tile roof. Also at the Fair Oaks end, the north parapet of the bridge slopes downward, then curves to meet a tall ornamental obelisk of reinforced concrete. On the obelisk's surface geometric shapes elaborated in concrete are applied to the surface. On each side of the west end, Oaklawn Avenue terminus, the parapet wall abuts into a curved buttress.

In October 1906, after the bridge was operational, a shoring pier at the center of the main span was added at the insistence of the Atcheson Topeka & Santa Fe Railway and adamantly opposed by the Architects. The design and construction of this shoring pier has not been attributed to the Architects. There was an attempt to match the character of the other piers, but the curve at the top of the added pier creates an asymmetrical shape that was not in keeping with the original arch spans, and a recess rather than a thru-hole appears on each side of the added shoring pier. The addition was always considered a non-contributing element. A U.S. Coast and Geodetic marker dated 1934 is implanted at its base, marking the San Bernardino Base Line which runs through the bridge.

The bridge has suffered seismic damage over time and successive tremors contribute to the bridge's hazardous state. The structure was closed to automobile traffic in 1973. It is now exclusively a pedestrian walkway. At the present time horizontal and longitudinal rebars appear deformed and at some points are exposed. The structure also exhibits deterioration in the roadway paving whose topping is uneven and abraded. Bridge wall surfaces have weathered and show some concrete spalling. Some of the bridge's vertical surface is covered with vines. Concrete has been patched and graffiti painted over with non-matching color and material. However, the Oaklawn Bridge has generally kept substantial integrity of materials and association, and retains the feeling of the austere composition and spare ornamentation characteristic of the Greene & Greene design.

The Waiting Station was restored to its original condition in 1997, but the missing rear wall was not reconstructed. Replacement tiles matching the original were custom made by the original manufacturer, the Ludiwici Tile Co. The City of South Pasadena proposes to seismically retrofit and restore the Oaklawn Bridge in 2001 to its original design according to the Department of the Interior's Standards and Guidelines for Historic Structures.

United States Department of the Interior
National Park Service

National Register of Historic Places Continuation Sheet

Los Angeles County, California

Section number 8 Page 1 Name of Property Oaklawn Bridge and Waiting Station

SIGNIFICANCE

The original 1972 National Register Form "Areas of Significance" checked the boxes for aboriginal historic, architectural, engineering, landscape architecture, transportation, and urban planning. Under the statement of significance, short paragraphs were written to support each of the categories. Based on those limiting statements, the Criterion A and C were the basis for the significance of the property.

Criterion A.

The original statements are brief and do not develop an adequate argument to support the facts that the property is associated with events that have made a significant contribution to the broad patterns of our history under the areas of aboriginal historic, landscape architecture, transportation and urban planning. The following are directly from the 1972 National Register Form

"Historically - Worthy of special note because it is the only bridge designed by Greene and Greene, internationally known architects." This should be considered as part of the Criterion C significance.

"Transportation - This bridge for vehicle and foot traffic, serves a vital link between Oaklawn Avenue and Fair Oaks Avenue, spanning the Santa Fe Railway. The waiting station was intended for shelter for the electric car passengers."

"Urban Planning - It continues, after nearly 66 years, to be as effective and useful as the day it was built."

The bridge did not contribute to the broad patterns of our urban planning or transportation history. The South Pasadena Realty and Improvement Company built the bridge as access to the Oaklawn Place subdivision of 24 houses. Oaklawn Place is the main and only street through the subdivision, which is bounded by Freemont Avenue, on the west, Foothill Street on the South, Fair Oaks Street on the East, and Columbia Street on the North. Fair Oaks Avenue was the major street through the area, which included the Pasadena Electric Short Line to Los Angeles. The bridge provides access from Fair Oaks Avenue to one end of Oaklawn Place and a set of arroyo stone porticos created the entrance to the other end of Oaklawn Place at Columbia Street. The bridge and Oaklawn Place only service the small subdivision and do not extend beyond Fair Oaks Avenue on the east or Columbia Avenue on the north.

The bridge was a sales feature to attract potential buyers for the Oaklawn Subdivision lots. The developers promotional brochure, ca 1907, "Oaklawn, A Suburb de Luxe" states "Build you a house in a garden of homes. The pure air and sunshine of the country, the abundant trees, the sweeping lawns and flowers, offer a setting for homes, with all of the city comforts and conveniences, which is unexcelled anywhere in the Italy of the American continent. Within one block of the Hotel Raymond and the Pasadena Electric Short Line to Los Angeles. The stores and office buildings and theaters are scarcely twenty minutes distance." The small waiting station structure at the east end of the bridge was a place for the residents to wait for the electric rail car on Fair Oaks Avenue.

The bridge is significant for only for its architecture related to Greene and Greene and is not significant in relationship to the broad patterns of our history under the areas of aboriginal historic, transportation and urban planning.

United States Department of the Interior
National Park Service

National Register of Historic Places Continuation Sheet

Los Angeles County, California

Section number 8 Page 2 Name of Property Oaklawn Bridge and Waiting Station

SIGNIFICANCE (cont.)

Criterion C - Additions to Original National Register Nomination

The Oaklawn Bridge and Waiting Station are significant in the area of design and architecture because they were designed by the Pasadena firm of Greene & Greene Architects, famous for their elaborate, highly artistic, organic style bungalows, and engineered by Michael de Palo, and Italian expert in early reinforced concrete. This was the only bridge designed by the Greene Brothers, and the Waiting Station is one of the most elaborate of their small structures. They were constructed in 1905 for the South Pasadena Realty and Improvement Company to shorten the distance from Fair Oaks to the Oaklawn Estates and as an amenity intended to help sell lots in the small subdivision.

The Greene brothers' influence has been widely documented, refining the American Bungalow to a fine art with its organic quality integrating inside and out. Architectural historian Reymer Banham has credited them with influencing residential architecture in the western United States in the early twentieth century more than any other architects. The firm produced their major work from 1903 until 1914. In 1952, the American Institute of Architects presented Charles Sumner Green and Henry Mather Greene with a citation in which they were called: "Formulators of a new and native architecture".

The bridge reflected the California style pioneered by these architects rather than the traditional European bridge design, and the collaboration of engineer Michael de Palo made the bridge an engineering achievement of its day. Described by the Los Angeles Daily Times on July 8, 1906 as "a graceful bridge, the most extensive reinforced concrete structure yet undertaken in the country," the bridge is reported by Greene & Greene architectural historian Edward Bosley to have been inspired by a neo-Palladian bridge seen by Charles S. Green on a visit to the 18th century garden of Stourhead, Wiltshire in England.

The design of the bridge was altered in October 1906 because haunch cracks appeared when the shoring was removed from under the arches. The Atchison Topeka & Santa Fe Railway insisted on an added pier at the center of the largest span over their tracks even though field-testing demonstrated that the bridge would perform as designed. This addition interrupted the graceful form of the bridge, and Pasadena Star reported that the city should beautify the structure by planting "ficus repens" profusely around the structure, making it less unsightly than at present. No evidence could be found to document the involvement of Greene & Greene in the design of the added pier. Record drawings were acquired by the restoration Project Architect Dan Peterson, A.I.A., who also has the drawing sheet for the shoring pier. He reports that all bridge drawings are stamped "Greene & Greene." However, the drawing for the added pier has no signature and is not in the same hand as the bridge drawings. Edward Bosley, in his monograph titled "Greene & Greene" states that the extra pillar was "conceived by committee after the fact". The Greenes subsequently disassociated themselves from the entire Oaklawn project and were replaced by another architect in 1907.

The Waiting Station has been called an "amazing concoction of redwood beams with tile roof" which is similar to the Oaklawn Gates also designed in 1905. The design is more in keeping with the architects' organic style using large river boulders from the nearby Arroyo Seco as walls and pillars for the intricate heavy exposed timbers and beautiful tile roof. This Craftsman shelter on the Fair Oaks Avenue end of the bridge was used to wait for the "Big Red Cars" on the Pacific Electric line - a branch of what was once one of the greatest rapid transit systems in the country.

United States Department of the Interior
National Park Service

National Register of Historic Places
Continuation Sheet

Los Angeles County, California

Section number 9 Page 1 Name of Property Oaklawn Bridge and Waiting Station

Bosley, Edward R. *Greene & Greene*. In proof, Phaidon Press, [June 2000].

"Bridge Link in Pasadena Road," *Los Angeles Daily Times*, July 8, 1906.

Gebhardt, David et. al. *Architecture in Los Angeles*. Peregrine Smith Inc.; Salt Lake City, 1984.

Girvigian F.A.I.A., Raymond. letter to California State Preservation Officer Dr. Knox Mellon. Oaklawn Bridge & Waiting Station, May 1, 2001

Lee, Portia. "Historic Resources Evaluation Report and Finding of No Adverse Effect for Oaklawn Bridge, City of South Pasadena Seismic Retrofit and Historic Restoration Project". Department of Public Works, South Pasadena, March 2000.

Makinson, Randell L., *Greene & Greene: Architecture as Fine Art*, Peregrine Smith, Inc.; Salt Lake City, 1994.

McCoy, Esther. *Five California Architects*, Praeger Publishers, Inc.; New York, 1975.

National Register Application: Oaklawn Bridge and Waiting Station, 1973

"Oaklawn Bridge Record Drawings - 1905", Greene & Greene Library at the Huntington Library, San Marino, CA

"Vines to Cover Big White Bridge," *Pasadena Star*, October 11, 1906

United States Department of the Interior
National Park Service

National Register of Historic Places Continuation Sheet

Los Angeles County, California

Section number 10 Page 1 Name of Property Oaklawn Bridge and Waiting Station

Verbal Boundary Description

The Oaklawn Bridge is a 340 ft. long span that extends WNW from an approximate 70 ft. wide entrance at the sidewalk at Fair Oaks Avenue to an approximate 33 ft. opening at Oaklawn Avenue. Center spans are approximately 20 ft. wide with the two end spans curving gracefully to form the wider terminises at Fair Oaks and Oaklawn. The frontage on Fair Oaks Avenue includes the approximately 12 foot by 20 foot Waiting Station at the southeast parapet flange and the ornamental obelisk at the northeast parapet flange.

Boundary Justification

The boundaries described above delineate the footprint of the bridge and waiting station structures and do not relate to parcel boundaries that contain the historic structures.







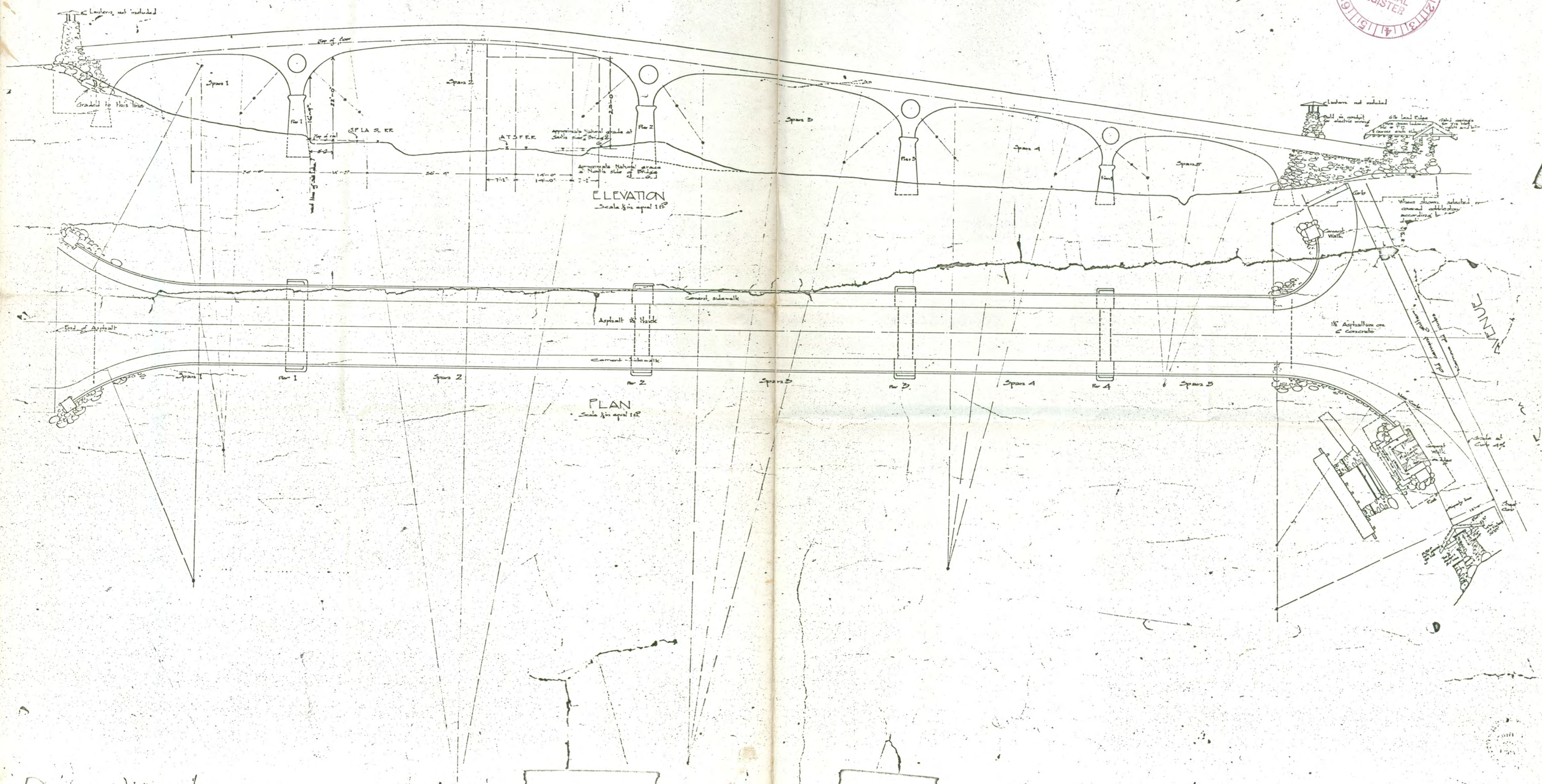
Alfred
-e-
Sharon
-68-



REINFORCED CONCRETE BRIDGE AT OAKLAWN SO PASADENA CALIF.
FOR
SOUTH PASADENA REALTY AND IMPROVEMENT COMPANY
GREENE & GREENE ARCHTS. 722 GRANT BUILDING LOS ANGELES CALIF.
M. DE PALO CONSULTING ENGINEER.

SHEET NO 1

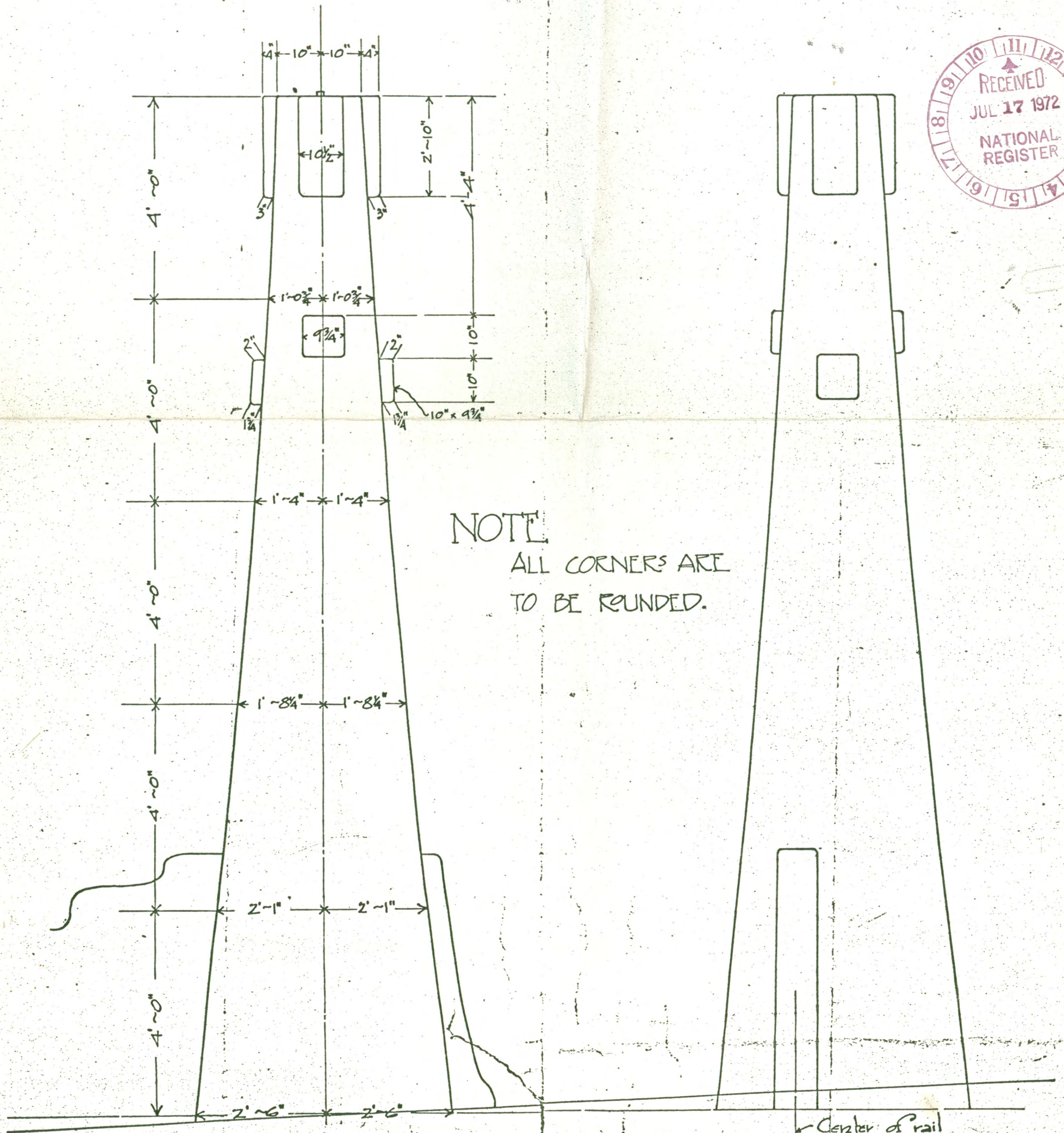
GREENE & GREENE LIBRARY
THE CAMPBELL PAPER



DETAIL OF PIERS AT EAST END OF BRIDGE.

Scale $\frac{1}{2}$ in. equals one foot.

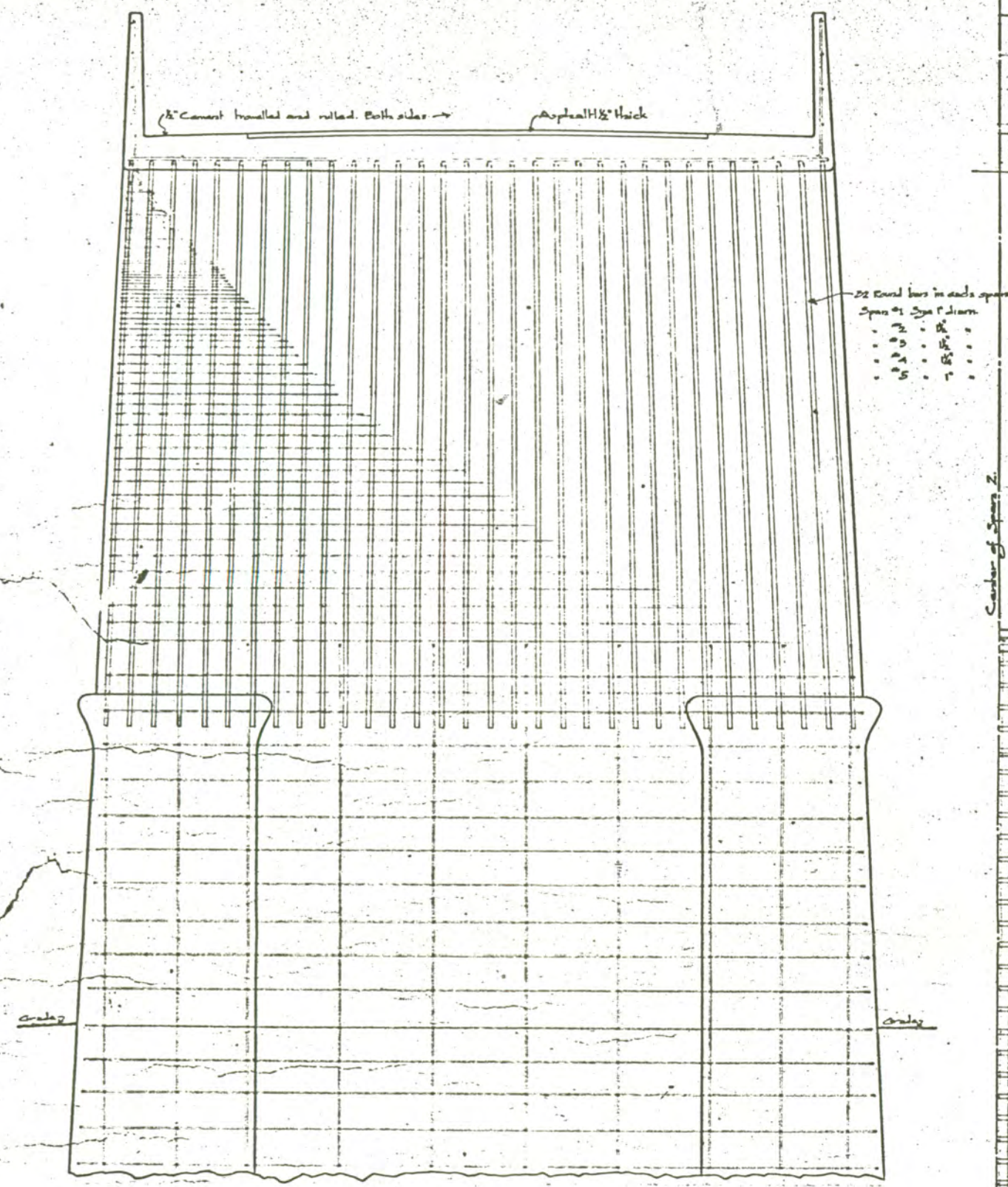
SO. PASADENA REALTY & IMPROVEMENT CO. SO. PASADENA
SHEET NO 5
JUNE 23rd '06



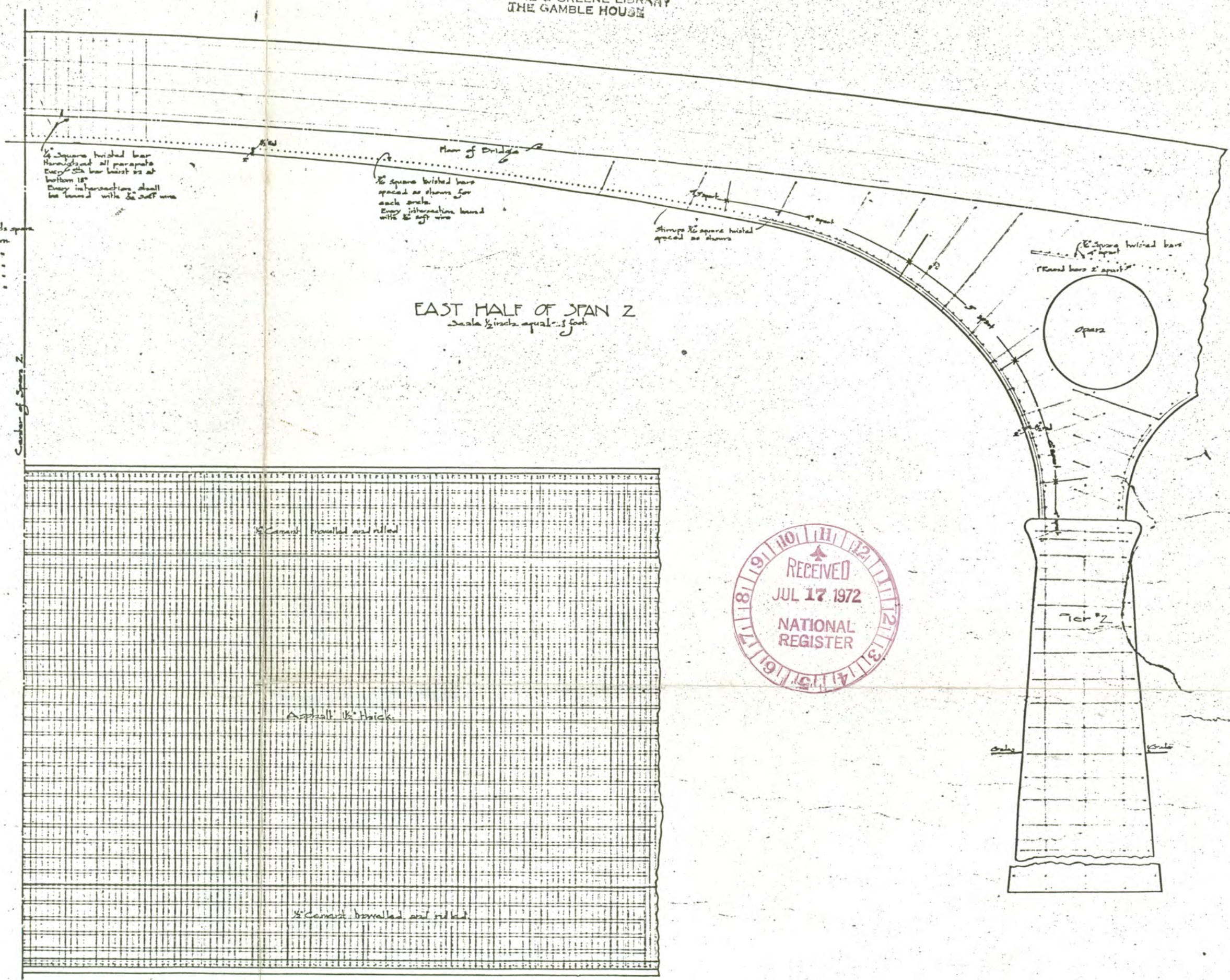
NOTE
ALL CORNERS ARE
TO BE ROUNDED.

SQUARE IN PLAN.

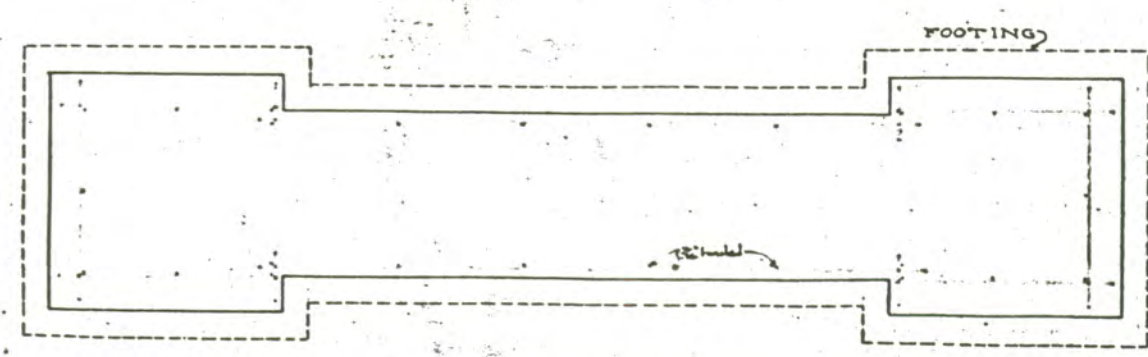
Center of rail
of bridge



CROSS SECTION ON CENTER LINE, LOOKING EAST.
Scale 1/2\"/>



EAST HALF OF SPAN 2
Scale 1/2\"/>

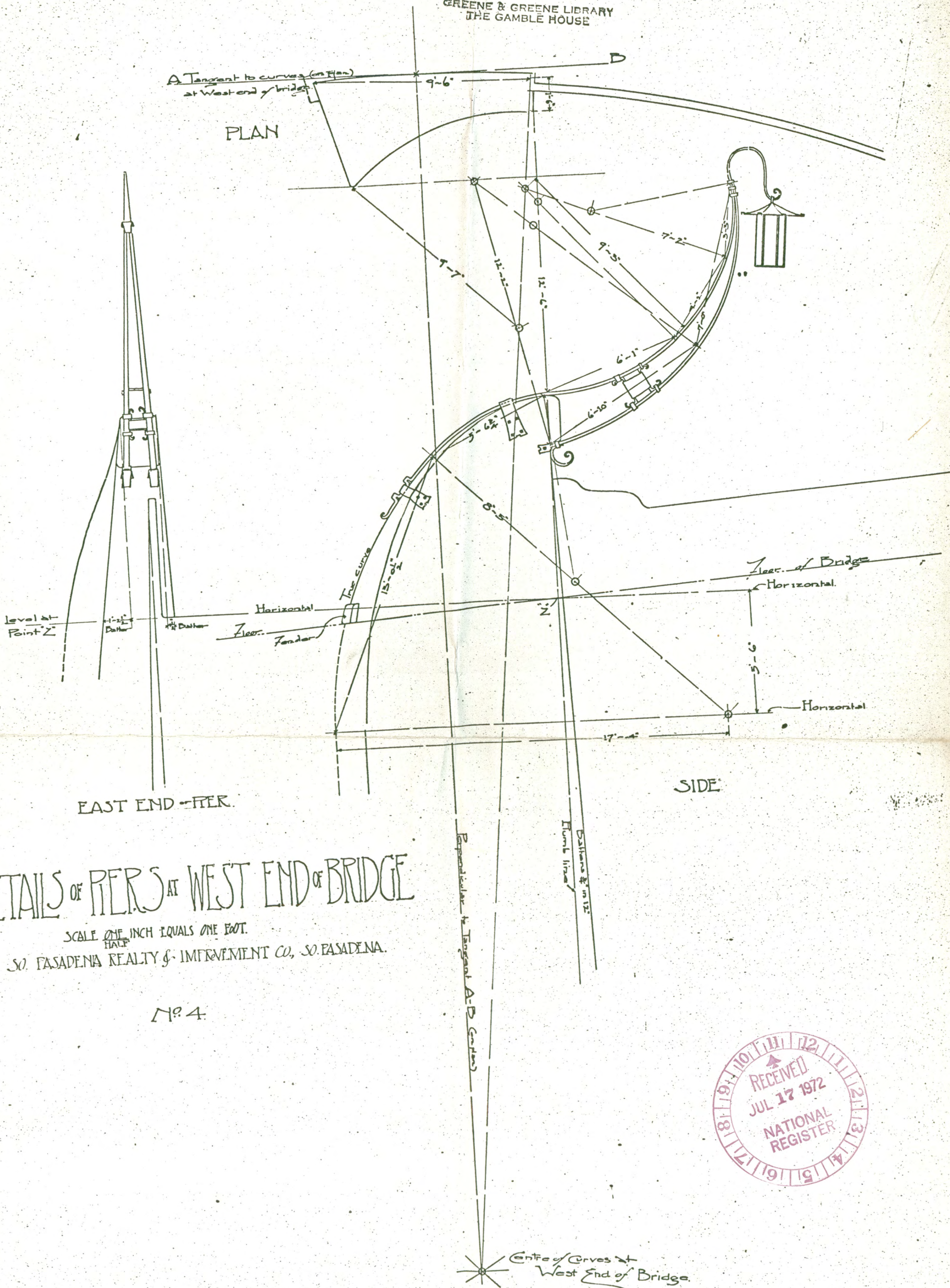


PLAN THRO PIER
Scale 1/2\"/>

REINFORCED CONCRETE BRIDGE AT OAKLAWN, SO. PASADENA, CALIF.
FOR
SOUTH PASADENA REALTY AND IMPROVEMENT COMPANY.
GREENE AND GREENE ARCHTS, 722 GRANT BUILDING, LOS ANGELES CALIF.
M. DE PALO, CONSULTING ENGINEER.

SHEET NO 2.





DETAILS OF PIERS AT WEST END OF BRIDGE

SCALE ONE INCH EQUALS ONE FOOT.
HALF
S. PASADENA REALTY & IMPROVEMENT CO., S. PASADENA.

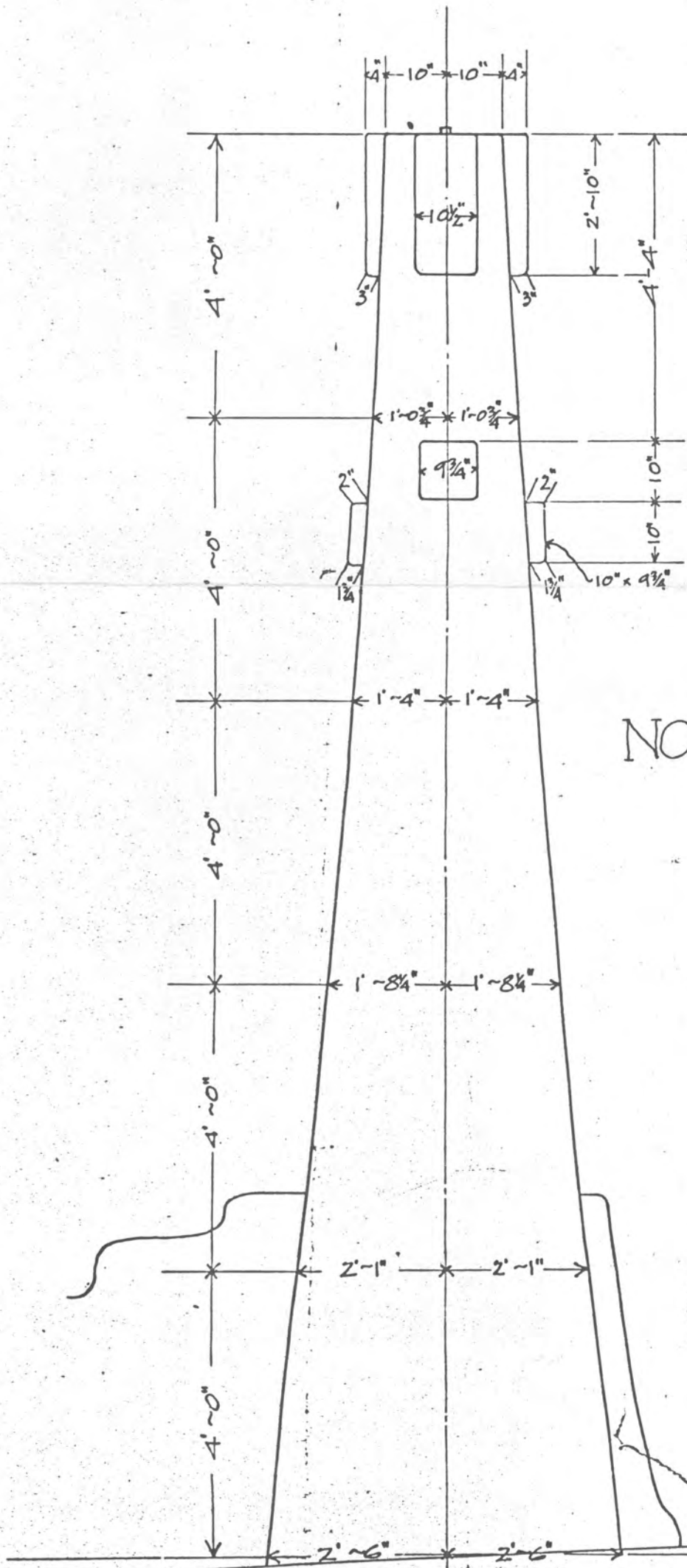
No. 4.



DETAIL OF PIERS AT EAST END OF BRIDGE.

Scale $\frac{1}{2}$ in. equals one foot.

SO. PASADENA REALTY & IMPROVEMENT CO. SO. PASADENA
SHEET NO. 5
JUNE 23rd '06



NOTE

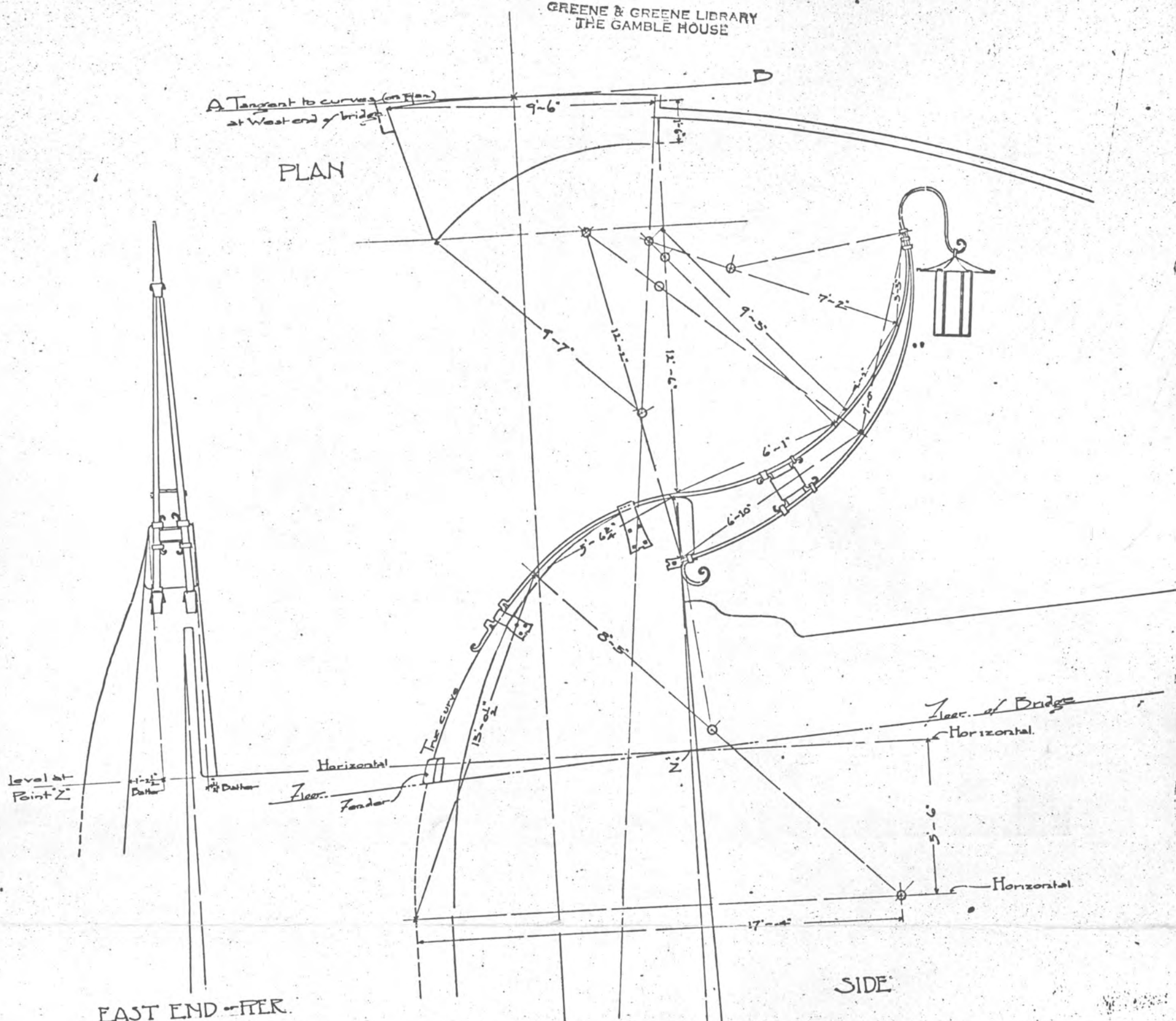
ALL CORNERS ARE
TO BE ROUNDED.

SQUARE IN PLAN.

Center of rail
of bridge

11

GREENE & GREENE LIBRARY
THE GAMBLE HOUSE



DETAILS OF PIERS AT WEST END OF BRIDGE

SCALE ONE INCH EQUALS ONE FOOT.
HALF
S.O. PASADENA REALTY & IMPROVEMENT CO., S.O. PASADENA.

No. 4.



Centre of Curves at West End of Bridge.

DEPARTMENT OF PARKS AND RECREATION

P.O. BOX 2390
SACRAMENTO 95811



Came in
7/17/72

October 27, 1972

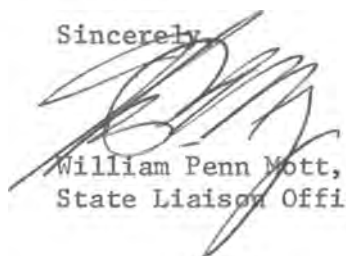


Mrs. Elizabeth Archanbeault, Editor
National Register of Historic Places
U. S. Department of the Interior
1100 L Street, N. W.
Washington, D. C. 20006

Dear Mrs. Archanbeault:

We are enclosing a corrected description sheet for inclusion in the pending National Register application for Adobe Flores, South Pasadena, Los Angeles County, California. The corrected sheet was initiated by the sponsor of the application to clarify a point regarding alteration of the structure. The enclosed information supersedes the description formerly submitted under Item 7 of the National Register Nomination form.

Sincerely,


William Penn Mott, Jr.
State Liaison Officer

Enclosure

E-4/11

11/6/72

DEPARTMENT OF PARKS AND RECREATION

P.O. BOX 2390
SACRAMENTO 95811



November 10, 1972

Dr. William J. Murtagh
Keeper of the National Register
U. S. Department of the Interior
18th and C Streets, N. W.
Washington, D. C. 20240

Attention: Mr. Jerry Rogers

Dear Dr. Murtagh:

As State Liaison Officer for the National Historic Preservation Act, I request that a correction be entered on a pending application for the National Register of Historic Places. The application is for the Oak-lawn Bridge and Waiting Station in South Pasadena, Los Angeles County, California. The change was initiated by the sponsor and involves Item 6, "Representation in Existing Surveys".

The following information supersedes information originally shown under Item 6 of the Inventory-Nomination Form:

Title of Survey:	Architect's Original Drawings
Date of Survey:	1906, Local
Depository for Survey Records:	Engineering Department of South Pasadena 1424 Mission Street South Pasadena, California
Code:	06

Sincerely,

A handwritten signature in cursive script, appearing to read "Robert H. Mott".
William Penn Mott, Jr.
Director



JAN 29 1973

H34-PHR

Mr. William Penn Mott, Jr.
Director
Department of Parks and Recreation
State Resources Agency
Post Office Box 2390
Sacramento, California 95811

Dear Bill:

We are returning the nomination forms for the following properties: Flores Adobe, Oaklawn Bridge and Piute Pass. These three nominations have the common omission that no date was filled in at the time you signed the nomination. This is particularly important in case a legal question should arise in connection with a property. While this may appear a picayune request, we have already had litigation which revolved upon this very omission and are therefore sensitive to it.

Please note also that both the Oaklawn Bridge and the Piute Pass nominations require other corrections. Details are included on the sendback sheet with each property.

We will be happy to reconsider the nominations as soon as the corrected forms are resubmitted.

Sincerely yours,

William J. Murtagh
Keeper of the National Register

Enclosures

Director, Western Region
LI- Mr. Winge
PHR -
PHR - Mrs. Mott w/cy inc
CSHULL:so-p 1/29/73 BASIC FILE RETAINED IN PHR

DEPARTMENT OF PARKS AND RECREATION

P.O. BOX 2390
SACRAMENTO 95811



June 20, 1973

Dr. William J. Murtagh
Keeper of the National Register
U.S. Department of the Interior
18th and C Streets, N.W.
Washington, D.C. 20240

Attention Mr. Jerry Rogers

Dear Dr. Murtagh:

We are resubmitting the National Register application for the Oaklawn Bridge and Waiting Station, which was recently returned to our office for additional work.

We have recomputed the latitude and longitude, and have entered the corrected figure in the appropriate space on the application. We are, however, puzzled by the statement, which requests submission of an original U.S.G.S. Topographic Quad showing an identified system of coordinates; an original copy of the Los Angeles Quadrangle has accompanied the application since it was first submitted and is enclosed again in the present resubmission.

We hope the enclosed information will enable you to complete your review for the Oaklawn Bridge and Waiting Station.

Sincerely,

John H. Michael
John H. Michael, Supervisor
History Preservation Section

Enclosure

M-3/7





CITY OF SOUTH PASADENA
PUBLIC WORKS DEPARTMENT
1414 MISSION STREET, SOUTH PASADENA, CA 91030
TEL: 626.403.7240 • FAX: 626.403.7241
WWW.CI.SOUTH-PASADENA.CA.US

RECEIVED

MAY 08 2001

OHP

May 3, 2001

Dr. Knox Mellon, SHPO
California Department of Parks and Recreation
Office of Historic Preservation
P.O. Box 942896
Sacramento, CA 94296-0001

Re: Oaklawn Bridge National Register Amendment
City of South Pasadena

Dear Dr. Mellon.

The City of South Pasadena owns the 1906 Oaklawn Bridge and is prepared to enter into a construction contract to restore the bridge as Greene and Greene Architects designed it utilizing various sources of funding. We understand that to accomplish this goal and retain the funding, the National Register listing needs to be amended to clarify its significance based on new information.

As an elected representative of the community of South Pasadena, and the City of South Pasadena as owner of the bridge, I request that the 45 day review period be waived in order for the amendment to be heard by the California Historic Resources Commission at their May 17, 2001 meeting.

Your cooperation is very important for the success of the project.

Sincerely,

David V. Rose
Mayor

cc. Glenn Duncan, Chairman, Cultural Heritage Commission
Cynthia Howse, Staff Historian, Office of Historic Preservation
Dan Peterson, AIA Historical Architect, Project Architect



Cy

CITY OF SOUTH PASADENA

1414 Mission Street • South Pasadena • California 91030 • Tel: (626) 403-7220 • Fax: (626) 403-7221

CULTURAL HERITAGE
COMMISSION

Friday May 4, 2001

RECEIVED

MAY 09 2001

OHP

Dr. Knox Mellon, SHPO
California Department of Parks and Recreation
Office of Historic Preservation
PO Box 942896
Sacramento, CA 94296-0001

Re: Oaklawn Bridge Restoration Amendment, City of South Pasadena

Dear Dr. Mellon,

For more than a year, the South Pasadena Cultural Heritage Commission has been working to see the National Register listed Oaklawn Bridge restored to its original Greene & Greene design. This has always been with the understanding that the inappropriate and apparently unnecessary middle pier can be removed without endangering its safety.

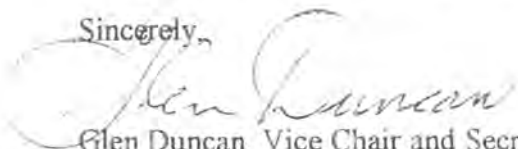
We urged the City to apply for a Getty "Preserve LA" grant to help assure that funds would be available to complete the restoration and organized the ad hoc oversight committee (see Ray Girvagian's exhibit K) to assure a consensus of expert opinion. On April 20 we approved a Certificate of Appropriateness for the project, to include removal of pier 1A if such removal would not result in loss of the National Register listing.

Time constraints of having to start the project no later than June 13 to hold the MTA funding (and complete bridge restoration without delaying Blue-Line construction) make amendment of the National Register application/listing a substantial preservation exigency.

On behalf of the Cultural Heritage Commission and the ad hoc Oaklawn Bridge Restoration Subcommittee, I therefore respectfully request that the State Historical Resources Commission hears the proposed National Register amendment at their May 17, 2001 meeting and any approvals forwarded for expedited review by the Keeper.

Thank you for your assistance with this important preservation project.

Sincerely,


Glen Duncan, Vice Chair and Secretary

cc. Cythia Howse, Staff Historian Office of Historic Preservation
Dan Peterson, AIA Historical Architect, Project Architect

**OFFICE OF HISTORIC PRESERVATION
DEPARTMENT OF PARKS AND RECREATION**

P.O. BOX 942896
SACRAMENTO, CA 94296-0001
(916) 653-6624 Fax: (916) 653-9824
calshpo@ohp.parks.ca.gov



May 23, 2001

Ms. Carol D. Shull, Keeper
National Register of Historic Places
National Park Service
US Department of the Interior
PO Box 37127
Washington, DC 20013-7127

Subject: **Oaklawn Bridge and Waiting Station
South Pasadena, Los Angeles County, California
Request to Amend a National Register Nomination**

Dear Ms. Shull:

Enclosed please find a revised **Oaklawn Bridge and Waiting Station** National Register nomination. This property is located in South Pasadena, Los Angeles County, California and was listed in the National Register in 1973.

Charles and Henry Greene designed the bridge and waiting station in 1905 for a new subdivision. The bridge carried local traffic over railroad tracks. Soon after the bridge was completed, it started cracking and, at the insistence of the Santa Fe Railway, a shoring pier was added for strength. It is not known who designed the shoring pier, but it is generally thought it was not the Greenes. It is recorded the Greenes were "devastated" by the additional shoring pier.

The 1973 statement of significance does not meet current documentation standards. The material states the property is associated with 20th century architecture, engineering, landscape, transportation, and urban planning histories, suggesting the property is perhaps significant under Criteria A and C in these various histories. NPS National Register staff acknowledge the documentation is vague and inconclusive regarding why the property is eligible for the National Register.

At the recommendation of the NPS National Register staff, an amended nomination was submitted to the California State Historical Resources Commission on May 17, 2001. The purpose of the amendment was to clarify why the property was significant and eligible for the National Register. The Commission found, based on the revised nomination, the property was significant at the local level under Criterion C for its physical characteristics in the area of architecture as the work of master architects Charles and Henry Greene. The property was found not eligible under Criterion A for

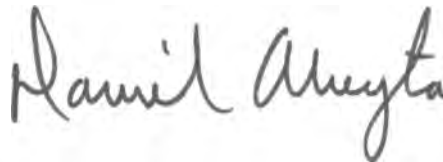
Ms. Carol D. Shull, Keeper
May 23, 2001
Page Two

historical associations in the areas of transportation and urban planning. The bridge carried local traffic into and out of a small subdivision. It was not the only access to the subdivision. Therefore the bridge did not have a significant impact on the city's transportation and urban planning histories.

The amended nomination was submitted to OHP after the federal deadline to notify local governments and property owners of the public hearing. All parties waived their full review time and consented to having the revised nomination heard by the Commission on May 17th.

If you have any questions regarding this revised nomination, please contact Cynthia Howse, of my staff, at (916) 653-9054.

Sincerely,

A handwritten signature in dark ink, appearing to read "Daniel Mellon". The signature is fluid and cursive, with the first name "Daniel" and last name "Mellon" clearly distinguishable.

 Dr. Knox Mellon
State Historic Preservation Officer

Enclosures



RAYMOND GIRVIGIAN, F.A.I.A.

historical architect

1401-B FAIR OAKS AVENUE • P.O. BOX 220 • SOUTH PASADENA, CALIFORNIA 91031 • 323 (213) 602-3848

Fax No. 626/799-4048

consultation
research
planning
restoration

Date: June 7, 2001

TRANSMITTAL:

TO: Paul R. Lusignan, Natl.Regis.Historian for Calif.
FROM: Raymond Girvigian, Ad Hoc Comm. for Oaklawn Bridge
PROJECT: OAKLAWN BRIDGE RESTORATION
SUBJECT: NATIONAL REGISTER AMENDMENT NOMINATION

REMARKS: VIA EXPRESS MAIL

DEAR MR. LUSIGNAN:

Enclosed is a copy of my May 1, 2001 submission to Dr. Knox Mellon, SHPO for California, requesting an amendment to the original nomination form (which I initiated 30 years ago) to reclassify the prime criteria for listing of the Oaklawn Bridge and Waiting Station under Criteria "C" (architectural) and deleting Criteria "A" (engr./transp.).

Said request was approved by the State Historic Resources as indicated on the attached letter from Dr. Mellon dated June 5, 2001, as you now know. Because the City of South Pasadena must proceed with a construction contract under a deadline (to keep federal highway funds) I respectfully request a brief faxed memo from you stating that the Keeper has approved relisting of my request as noted. I have heard informally by phone that your office has so approved the relisting, but I would appreciate a brief note to that effect.

If this request is in order, please fax your statement or a copy of the one you forward to SHPO to my office:

FAX: 626/799-4048.

Thanks for your courtesies in this regard.

RAYMOND GIRVIGIAN, FAIA
Consulting Historical Architect (AD HOC COMMITTEE, OAKLAWN BRIDGE)

c:
encl.: (2) Lttrs., & Copy of 5/ 01/01 SHPO submission

STATE OF CALIFORNIA - THE RESOURCES AGENCY

GRAY DAVIS, Governor

OFFICE OF HISTORIC PRESERVATION
DEPARTMENT OF PARKS AND RECREATION
P.O. BOX 942886
SACRAMENTO, CA 94296-0001
(916) 653-6624 Fax (916) 653-9824
calshpo@ohp.parks.ca.gov
www.ohp.calparks.ca.gov



June 5, 2001

Mr. Raymond Girvigian, FAIA
1401-B Fair Oaks Ave.
PO Box 220
South Pasadena, CA 91030

Dear Mr. Girvigian:

The Office of Historic Preservation acknowledges receipt of your letter of May 1, 2001 requesting an amendment to delete Criterion "A" from the National Register of Historic Places status of the Oaklawn Bridge and Waiting Station, a National Register property located in South Pasadena, Los Angeles County.

The amendment request was heard by the State Historical Resources Commission at a regularly scheduled meeting of the Commission on May 17, 2001 in San Diego. The Commission unanimously approved to accept the amendment to remove Criterion "A" status from the Oaklawn Bridge National Register designation.

Please do not hesitate to contact Eugene Itogawa at (916) 653-8936 should you require further assistance in this matter.

Sincerely,

A handwritten signature in dark ink, appearing to read 'Knox Mellon'.

Dr. Knox Mellon
State Historic Preservation Officer

STATE OF CALIFORNIA—BUSINESS AND TRANSPORTATION AGENCY

GRAY DAVIS, Governor

DEPARTMENT OF TRANSPORTATION

DISTRICT 7, 120 SO. SPRING ST.
LOS ANGELES, CA 90012-3606
(213)-897-0782



June 6, 2001

Raymond Girvigian
Historical Architect
1401 B Fair Oaks Avenue
South Pasadena, CA 91031

Dear Mr Girvigian:

In response to your question regarding TEA funding for the Oaklawn Bridge seismic retrofit and historic rehabilitation project, the TEA money will not be compromised by removing the pier provided the Keeper of the National Register agrees with the findings of the California Historic Resources Commission that the pier does not contribute to the bridge's historic significance.

Regards,

A handwritten signature in cursive script, appearing to read "Diane Kane".

Diane Kane, Ph.D.
Heritage Resource Coordinator
Caltrans, District 7



RAYMOND GIRVIGIAN, F.A.I.A.

historical architect

1401 - B FAIR OAKS AVENUE - P.O. BOX 220 - SOUTH PASADENA, CALIFORNIA 91030 - 323 (213) 682-3848

consultation
research
planning
restoration

May 1, 2001

TO: Dr. Knox Mellon, State Historic Preservation Officer
RE: OAKLAWN BRIDGE & WAITING STATION Proposal and Justification to
Amend the 1972 National Register (N.R.) Nomination
(clarifying the Pier Addition as non-Contributing bridge element)

Dear Dr. Mellon, and to whom else it may concern:

I am writing to provide the reasons why the above subject N.R. form must be amended to properly reflect that the Oaklawn Bridge and Waiting Station's prime basis for listing on the 1973 National Register should be for **Criteria C, its architectural significance** rather than for any other (secondary) basis such as Criteria A, (history of transportation or urban planning, etc.). The after-thought pier addition (by other than the architects Greene and Greene) was mostly a public relations ploy to mitigate the negative publicity about a possible public safety issue, which was proved negligible. Though the pier addition was unnecessary, it was insisted upon by the Sta. Fe RR and most likely by the developer, the So. Pasadena Realty and Improvement Co., which built and owned it. The Greenses were dismayed and disassociated themselves from the entire Oaklawn Project (replaced by architect/developer G.L. Stimson in 1907). Consequently, the pier addition is not an integral part of the Greene's design - the prime basis for this nomination - and is therefore a **non-contributing element**. In-fact, it devalues the historical integrity of this unique landmark.

My official involvement with this matter began in 1971 when, as the founding chairman of the South Pasadena Cultural Heritage Commission, I initiated the nomination (**EXHIBIT-A**) by delegating the preparation and submission of the nomination to our city's Commission Vice Chair and Historical Subcommittee chair, Margaret Fay. By April of 1972, I received the nomination form (along with several others) to review, as the historical architect member of the State Landmarks Committee (which we later reorganized as the Historical Resources Commission). We were at the very beginning of the National Register Program in California with many duties other than the N.R. forms to review. Some errors and ambiguities apparently slipped by, which in this case I'll now explain:

Attached to the subject form (**EXHIBIT-B**) was DPR staff outline comments- probably by the late Aaron Gallup - (**EXHIBIT-C**) viz: "...Should be judged on **architectural significance solely**." Further down we may note: "Item 3 (C) - **Architecture - sole example**." Then under General Comments: "Staff recommendation: **Defer to Mr. Girvigan**."

POINT 1: I now reiterate, reaffirm and attest, after 29 years, that I agreed with staff's assessment then, and further state that the Oaklawn Bridge and Waiting Station was submitted, I thought, to be approved and listed on the basis of its architectural significance - as a unique and outstanding example of, and by its association with, the world renowned architects, Charles and Henry Greene, with pioneer engineer Michael de Palo. It is unique in style as an Early 20th Century structure - blending the spirit of meticulous design craftsmanship with a daring, graceful and subtle

RAYMOND GIRVIGIAN, F.A.I.A./historical architect

May 1, 2001

OAKLAWN BRIDGE and WAITING STATION - N.R. NOMINATION AMENDMENT

Page 2

beauty echoing L'Art Nouveau (still popular then) while presaging a modernity that was to come; a noteworthy advancement for its time.

POINT 2: The added pier by "others" was adamantly opposed by the Greenes, as amply documented by scholars of these architects in publications (e.g., Randall Makinson and Edward Bosley, members of our Ad Hoc Oversight Committee). The Greenes did not add the pier. I regret now that this point was not clarified previously as I show more fully in **POINT 3.** below. During the hectic, early N.R. Program days, we reviewed the first rush of nominations, some things did slip by that were not worth returning for correction that would delay the listing process, especially for cases that obviously merited approval. Who could have anticipated that such "technicalities" might later result in major repercussions? After 95 years, we now have the chance to respect the legacy of the designers, of whom the noted architectural historian, Reyner Banham, said in 1976, were among the few most influential architects of the 20th Century.

POINT 3: A. Please review the 1972 nomination (**EXHIBIT-B**) under "Description," 3rd paragraph, where it states, "...this plan of construction was not acceptable to the Santa Fe Railway Co., and consequently, an additional pillar (sic) was added...'They' added the center pillar reluctantly..." This is an unintended, but misleading, comment and needs now to be corrected. It has unfortunately resulted in an erroneous conclusion that the superfluous pier was indeed part of the original design, since it was added by the architects, albeit reluctantly. But let me attest to the fact that we have examined the documents and found no trace of the Greenes' names on the later pier drawings, as we have seen their stamps of identity clearly on the original sheets. Further, the style of the drawings are unlike the Greenes work, as is the design of the added pier itself.

B. We have documentation that the Greenes were adamantly opposed to the "shoring" pier and were "heart-broken" that it was eventually added by "others." Please refer, for example to **EXHIBIT-D**, a memo from Randell Makinson to me, that notes references to his publication and interviews by him and Edward (Ted) Bosley, another published Greene and Greene scholar.

C. After completion of the bridge, some stress cracks developed. **EXHIBIT-E** shows the bridge with some train smoke on the parapets, indicating it was up for a while before the later pier was added.

D. From newspaper accounts, negative publicity was generated about the cracks. The Sta. Fe RR, whose trains ran under the bridge, was surely involved in wanting to allay the public's fears of possible bridge failure. Examine the historical photo in **EXHIBIT-F**, which is a view before the added pier: from Fair Oaks Ave. to the newly developing Oaklawn Tract of fine homes - the main purpose of the bridge, as a gateway to this now designated historical district. Imagine the pressure on the developers to take some action quickly to reassure the public and avoid scaring off potential buyers

RAYMOND GIRVIGIAN, F.A.I.A./historical architect

May 1, 2000

OAKLAWN BRIDGE and WAITING STATION - N.R. NOMINATION ADMENDMENT

Page 4

Fair Oaks Avenue (remaining so to recent times; the main access and entry is at Columbia St.). It was a means by the Realty Company to enhance the desirability of the Oaklawn Tract; (see developers' tract brochure, "Oaklawn, A Suburb de Luxe", ca. 1907+?, **EXHIBIT-J**).

SUMMARY/CONCLUSION

The South Pasadena Realty and Improvement Co. retained the eminent architects Greene and Greene (a sales ploy in itself) to design a "modern" notable bridge to "hype" in its sales campaigns to exploit its driving purpose which was to develop the new suburban Oaklawn Tract. The developers hired the architects, paid for the bridge (\$13,500.: see Oaklawn tract map at the back of the brochure, Exhibit-J), and undoubtedly added the pier, when forced to because of adverse publicity and the Sta. Fe RR; the developers also owned it until soon after the alien pier was constructed, when the City of South Pasadena took over. It was built by the developers only for the above purpose, which is patently clear since its connection is only between Fair Oaks Avenue and Oaklawn Avenue homes; it doesn't lead to anywhere else and is actually a secondary access from Oaklawn Avenue to Fair Oaks Ave. (see Exhibit-J map).

Its prime N.R. significance is its association with the pre-eminent, Turn-of-the Century architects, the brothers Greene, and as a sole example of their architectural bridge design work; it is truly unique. The architectural significance of the Oaklawn Bridge and Waiting Station rests also on its imaginative and daring concept for an Early 20th Century design. Although the Bridge is a secondary link in urban planning and transportation, it is a **major link to the legacy of the brothers Greene**, one of the most influential and world renowned architects of the 20th Century, which takes precedence in this case over all other considerations for significance..

Thus, the added, redundant pier is both alien in concept and almost structurally useless. In the scope and spirit of my original N.R. nomination intent, the added pier cannot be considered as a contributing element of the Greenes' structure. If allowed to remain, it will continue to despoil the design integrity of the Bridge and the memory of the Greenes and their legacy. With the MTA inter-urban rail soon upon us in the old right-of-way under the bridge, this is our last opportunity to restore the Greenes' masterwork to the highest level befitting its significance and to meet the federal government's and the Getty Foundation's expectations for a restoration effort commensurate with that recognition.

Our Ad Hoc volunteer Committee of architectural historians and Greene and Greene experts (**Exhibit-K**) all unanimously urge the removal of the added pier. This unique landmark and the Greenes' legacy deserve nothing less than our best effort - without the later, alien, non-contributing pier addition. Thank your for consideration of this submission.

RAYMOND GIRVIGIAN, F.A.I.A./historical architect

May 1, 2001

OAKLAWN BRIDGE and WAITING STATION - N.R. NOMINATION AMENDMENT

Page 3

of its residential lots; so the redundant pier was added, whether or not it was needed. The developers had retained the Greenes to design the bridge and they now owned it; (EXHIBIT-G,...710 EIS/EIR Hist. Report). "They" were surely being pressed by the Sta. Fe RR to solve this "apparent" problem - at the expense of the Greenes' and the engineer's reputations.

E. Even though a dead-load of 18 tons at the crown of the major span showed a deflection of only 1/8", (EXHIBIT-H photo), the pier was added by "others" and the Greenes departed in dismay; it was in place by October of 1906 (Hist. Resources Evaluation Report, Dr. Portia Lee, Mar.2000; OHP file).

F. The added pier is not only out of character and style with the Greenes' design but was structurally unnecessary (in the context of its time) as evaluated by Michael Krakower, FASCE, the Structural Engineer for the current seismic retrofit project, who has analyzed both the original bridge drawings by the Greenes and the later pier addition drawings; (see his statement to me; (EXHIBIT-I)).

G. In summary, there is no documented evidence that, "they" (i.e., the Greenes) added the alien pier, reluctantly or otherwise. To the contrary, by evidence, family interviews and inference, quite the opposite. Therefore, the added pier is not an integral, or essential part of the Greenes' originally conceived and constructed bridge design - the basis of the N.R. nomination; consequently, it is not a contributing-element of the original structure nominated.

POINT 4. A. Under "8. Significance" of the 1972 nomination form, Margaret was beginning to have cataract trouble (she later became visually impaired) and she accidentally marked the box for "Agriculture" rather than the intended "ARCHITECTURE" box immediately below. It was not noticed until too late.

B. She also marked every box even remotely relevant thinking that this might facilitate the approval of the nomination. In this regard, on the form below, under "Statement of Significance," she briefly noted other secondary references, without weighting them in order of priority or strength. In this early N.R. submission such technical "errors" were overlooked since they were overcome by staff's comments (op.cit.): that the sole criterion was indeed architectural significance; staff then deferred judgment to me, and I concurred, as the reviewing historical architect.

C. I can further elaborate that the bridge has limited local historical value or influence on transportation or urban planning, except secondarily as part of the entire Oaklawn Historical District development. Nor did it notably influence successive engineering bridge design. It was really a sales feature to attract potential buyers of the adjacent Oaklawn Tract of residential lots (now an eligible N.R. Historic District). It was touted as a convenience feature for the Oaklawn residents, a secondary access to

Return to Gary Gavigan

Form 10-300
(July 1969)

UNITED STATES DEPARTMENT OF THE INTERIOR
NATIONAL PARK SERVICE

NATIONAL REGISTER OF HISTORIC PLACES
INVENTORY - NOMINATION FORM

(Type all entries - complete applicable sections)

STATE: California
COUNTY: Los Angeles
FOR NPS USE ONLY
ENTRY NUMBER
DATE

1. NAME
COMMON: Oaklawn Bridge AND WAITING STATION
AND/OR HISTORIC:

2. LOCATION
STREET AND NUMBER: Between Oaklawn and Fair Oaks Avenues
CITY OR TOWN: South Pasadena
STATE: California
CODE: 06
COUNTY: Los Angeles
CODE: 037

3. CLASSIFICATION			
CATEGORY (Check One)	OWNERSHIP	STATUS	ACCESSIBLE TO THE PUBLIC
<input type="checkbox"/> District <input type="checkbox"/> Site <input type="checkbox"/> Object	<input checked="" type="checkbox"/> Building <input type="checkbox"/> Structure <input type="checkbox"/> Both	<input checked="" type="checkbox"/> Public <input type="checkbox"/> Private <input type="checkbox"/> Both	Public Acquisitions: <input type="checkbox"/> In Process <input type="checkbox"/> Being Considered
PRESENT USE (Check One or More as Appropriate)		Comments	
<input type="checkbox"/> Agricultural <input type="checkbox"/> Commercial <input type="checkbox"/> Educational <input type="checkbox"/> Entertainment		<input type="checkbox"/> Government <input type="checkbox"/> Industrial <input type="checkbox"/> Military <input type="checkbox"/> Museum	
<input type="checkbox"/> Park <input type="checkbox"/> Private Residence <input type="checkbox"/> Religious <input type="checkbox"/> Scientific		<input checked="" type="checkbox"/> Transportation <input type="checkbox"/> Other (Specify)	

4. OWNER OF PROPERTY
OWNER'S NAME: City of South Pasadena
STREET AND NUMBER: 1424 Mission Street
CITY OR TOWN: South Pasadena
STATE: California
CODE: 06

5. LOCATION OF LEGAL DESCRIPTION
COURTHOUSE, REGISTRY OF DEEDS, ETC: Los Angeles Hall of Records
STREET AND NUMBER: 320 West Temple Street
CITY OR TOWN: Los Angeles
STATE: California
CODE: 06

6. REPRESENTATION IN EXISTING SURVEYS
TITLE OF SURVEY: Architects Design
DATE OF SURVEY: 1906
DEPOSITORY FOR SURVEY RECORDS: Greene & Greene Library
STREET AND NUMBER: The Gamble House #4 - Westmorland Place
CITY OR TOWN: Pasadena
STATE: California
CODE: 06

STATE: California
COUNTY: Los Angeles
FOR NPS USE ONLY
ENTRY NUMBER
DATE

7. DESCRIPTION
CONDITION
(Check One) <input type="checkbox"/> Excellent <input checked="" type="checkbox"/> Good <input type="checkbox"/> Fair <input type="checkbox"/> Deteriorated <input type="checkbox"/> Ruins <input type="checkbox"/> Unexposed
(Check One) <input type="checkbox"/> Altered <input type="checkbox"/> Unaltered <input type="checkbox"/> Moved <input checked="" type="checkbox"/> Original Site
DESCRIBE THE PRESENT AND ORIGINAL (If known) PHYSICAL APPEARANCE
<p>This bridge is a graceful, arched structure rising from an embankment at the west end curving to street level at the east, where a waiting station, constructed of boulders from the nearby Arroyo Seco, with a heavy wood-beamed roof of Ludowici clay tile, terminates at the south balustrade. A detail of red brick is incorporated with the boulders, a combination typical of the architects' work. Presently the bridge is covered with ivy, which is periodically trimmed.</p> <p>The bridge has six spans and a total length of 340 feet. The concrete arch is only twelve inches thick at the center. Reinforcing is twisted square bars of metal 7/16 of an inch in size and laid from 3'0" to 6 inches apart. Longitudinal bars 1 1/2 inches in thickness are used in other sections. The coping is 8 inches at the base and 4 inches at the top. Forty-five tons of steel and 20,000 cubic feet of concrete mark it as a pioneer of its day.</p> <p>Original sketch and plans for the bridge show five spans. This plan of construction was not acceptable to the Santa Fe Railway Co., and consequently, an additional pillar was added to the center of span #2, to satisfy the railway. Architects, Greene and Greene felt this was not necessary. They added the center pillar reluctantly because they felt it interfered with the grace of the span design and was not necessary for structural soundness. There are no lighting fixtures on the bridge altho they appear on the original plans.</p> <p>In 1934, a United States Geodetic seal was implanted at the base of the tall concrete pylon at the northeast corner of the bridge.</p> <p>*Imprinted on the Ludowici Tile: T 12 Pat. July-19-98 Pat. Mar. 17-03</p>

SEE INSTRUCTIONS

SEE INSTRUCTIONS

EXHIBIT-B

SEE INSTRUCTIONS
(SHOULD BE PRIORITY SEQUENCE) (2)

8. SIGNIFICANCE			
PERIOD (Check One or More as Appropriate)			
<input type="checkbox"/> Pre-Columbian	<input type="checkbox"/> 16th Century	<input type="checkbox"/> 18th Century	<input checked="" type="checkbox"/> 20th Century
<input type="checkbox"/> 15th Century	<input type="checkbox"/> 17th Century	<input type="checkbox"/> 19th Century	
SPECIFIC DATE(S) (If Applicable and Known) 1906			
AREAS OF SIGNIFICANCE (Check One or More as Appropriate)			
<input type="checkbox"/> Aboriginal	<input type="checkbox"/> Education	<input type="checkbox"/> Political	<input checked="" type="checkbox"/> Urban Planning
<input type="checkbox"/> Prehistoric	<input checked="" type="checkbox"/> Engineering	<input type="checkbox"/> Religion/Philosophy	<input type="checkbox"/> Other (Specify)
<input checked="" type="checkbox"/> Historic	<input type="checkbox"/> Industry	<input type="checkbox"/> Science	
<input type="checkbox"/> Agriculture	<input type="checkbox"/> Invention	<input type="checkbox"/> Sculpture	
<input type="checkbox"/> Architecture	<input checked="" type="checkbox"/> Landscape	<input type="checkbox"/> Social/Humanitarian	
<input type="checkbox"/> Art	<input type="checkbox"/> Architecture	<input type="checkbox"/> Italian	
<input type="checkbox"/> Commerce	<input type="checkbox"/> Literature	<input type="checkbox"/> Theater	
<input type="checkbox"/> Communications	<input type="checkbox"/> Military	<input type="checkbox"/> Transportation	
<input type="checkbox"/> Conservation	<input type="checkbox"/> Music		
STATEMENT OF SIGNIFICANCE			
<p>The foresight of the South Pasadena Realty and Improvement Co. in sponsoring a structure, such as the Oaklawn Bridge, shortly after the turn of the century is significant for the following reasons:</p> <p>Historically -- Worthy of special note because it is the only bridge designed by Greene and Greene, internationally known architects.</p> <p>Architecturally -- It is entirely free of the European styles in vogue in this part of the world, in the early 1900's, and it reflects the California style as conceived and brought to a flowering by these architects. The builder was Carl Leonardt.</p> <p>Engineering -- Michael de Palo, an Italian, who pioneered in reinforced concrete, was the consulting engineer. The construction and design of concrete and metal mark it as an engineering achievement of its day.</p> <p>Landscaping -- The fitting of the bridge and waiting station into its environment with the landscaping as planned by the architects is still of vital significance in the community.</p> <p>Transportation -- This bridge for vehicle and foot traffic, serves a vital link between Oaklawn Avenue and Fair Oaks Avenue, spanning the Santa Fe Railway. The waiting station was intended for shelter for the electric car passengers.</p> <p>Urban Planning -- It continues, after nearly 66 years, to be as effective and useful as the day it was built.</p>			

9. MAJOR BIBLIOGRAPHICAL REFERENCES			
<p>There are no major published works containing facts about this bridge and structure. A publication listed below contains a chapter on Charles Sumner and Henry Mather Greene.</p> <p>McCoy, Ester - <u>Five California Architects</u>. Reinhold Publishing Corp. 1960.</p> <p>Chapter on Greene & Greene - by Randall L. Makinson, Curator - Gamble House #4 Westmoreland Place, Pasadena, California.</p>			
10. GEOGRAPHICAL DATA			
LATITUDE AND LONGITUDE COORDINATES DEFINING A RECTANGLE LOCATING THE PROPERTY		LATITUDE AND LONGITUDE COORDINATES DEFINING THE CENTER POINT OF A PROPERTY OF LESS THAN TEN ACRES	
CORNER	LATITUDE	LONGITUDE	
	Degrees Minutes Seconds	Degrees Minutes Seconds	
NW	° ' "	° ' "	
NE	° ' "	° ' "	
SE	° ' "	° ' "	
SW	° ' "	° ' "	
APPROXIMATE ACREAGE OF NOMINATED PROPERTY: 2+			
LIST ALL STATES AND COUNTIES FOR PROPERTIES OVERLAPPING STATE OR COUNTY BOUNDARIES			
STATE:	CODE	COUNTY	CODE
STATE:	CODE	COUNTY:	CODE
STATE:	CODE	COUNTY:	CODE
STATE:	CODE	COUNTY:	CODE
11. FORM PREPARED BY			
NAME AND TITLE:			
Margaret Leslie Fay, A.I.D.			
ORGANIZATION			DATE
Vice-Chairman & Historical Chairman			
South Pasadena Cultural Heritage Commission.			April 11, 1972
STREET AND NUMBER:			
1424 Mission Street			
CITY OR TOWN:		STATE	CODE
South Pasadena		California	91030
			06
12. STATE LIAISON OFFICER CERTIFICATION		NATIONAL REGISTER VERIFICATION	
<p>As the designated State Liaison Officer for the National Historic Preservation Act of 1966 (Public Law 89-665), I hereby nominate this property for inclusion in the National Register and certify that it has been evaluated according to the criteria and procedures set forth by the National Park Service. The recommended level of significance of this nomination is:</p> <p>National <input type="checkbox"/> State <input type="checkbox"/> Local <input type="checkbox"/></p> <p>Name _____</p> <p>Title <u>State Liaison Officer</u></p> <p>Date _____</p>		<p>I hereby certify that this property is included in the National Register.</p> <p>Chief, Office of Archeology and Historic Preservation</p> <p>Date _____</p> <p>ATTEST:</p> <p>Keeper of The National Register</p> <p>Date _____</p>	

SEE INSTRUCTIONS

EXHIBIT-D

Randell L. Makinson

23 April 2001

To: Raymond Girvigian

From: Randell L. Makinson

Re: Oaklawn Bridge

South Pasadena Realty & Improvement Co.

233 North

Grand Avenue

Pasadena

California

91103 3522

Los Angeles Builder and Contractor / February 26, 1903:

The bridge now being constructed by Contractor Carl Leonardt on Ocean Ave, Santa Monica, is a new feature in bridge building in this section of the country, and is attracting considerable attention. The method of construction used is the Monier system, and the bridge has two sixty-six foot spans. The arches over the spans are of cement concrete, strengthened with steel. An essential feature of the construction is the comparative thinness of the cement arches.

All of the 100-foot width of the street is occupied by one of the arches and the other is being built fifty feet wide. Under the double-tracked trolley railway space the thickness of the cement arch will be only twelve inches. Under the walkway and driveway the thickness will be only six inches. Over the arches the space will be filled with earth to the level of the driveway.

Imbedded in the cement arches is a system of half-inch steel rods, criss-crossed and wired together

According to the engineers' reckoning the arch under the railroad space will support 450 pounds to the square foot and, under the driveway and sidewalk spaces, seventy-five pounds to the square foot.

Associates

[This is a bridge prior to the Oaklawn Bridge. Carl Leonardt was the contractor on the Oaklawn Bridge as well and his offices were down the hall from the Greene & Greene Los Angeles offices in the Pacific Electric building which Leonardt also built]

Los Angeles Times, February 08, 1906

.....Its promoters claim that it is destined to become one of the most popular passageways in the Pasadena district and that it is one of the most extensive reinforced concrete bridge structures undertaken in this country.....The general design of the bridge is by Architects Greene & Greene of Pasadena and Los Angeles, and the mechanical details are the work of Michael de Palo, an Italian expert in reinforced concrete work, who designed and constructed the long concrete single-arch bridge across the lagoon at Playa del Rey. De Palo will be consulting engineer in the Pasadena enterprise. Carl Leonardt will be the builder.....

Telephone

626 449 4600

Facsimile

626 449 2059

Randell L. Makinson

Personal Notes with Henry M. Greene daughter Isabelle Greene McElwain taken in 1957:

233 North

"The Greenes' were sorry they ever got mixed up in the deal regarding the bridge at Oaklawn. They had engaged an engineer to work with them on this job. The original design had one long arch over the railroad tracks. In construction the steel stirrups were not placed in right and when supports were removed it cracked but did not fail. The Santa Fe Railroad over which they had worked insisted that an additional pier be placed in the middle of the long arch and this broke Henry's heart. He never quite got over this disappointment. The bridge was quite a revelation in its time. He expressed that 'we really went through hell over that bridge'."

Grand Avenue

Pasadena

California

91103 3522

[The above is from notes taken in 1957 of a conversation with Isabelle Greene about comments made throughout his life by her father Henry M. Greene. Similar comments were made over the years of my research by other members of the family.]

Personal recollection:

[Charles Greene's son Nathaniel Patrickson Greene also discussed throughout the years that his father, Charles S. Greene, equally was totally dismayed by the addition of the pier to the design of the bridge, and he too never got over it.]

This subject has been included in my own publications on Greene & Greene as follows:

Greene & Greene, Architecture as a Fine Art, by Randell L. Makinson, Peregrine Smith Books, 1977, Pages 113, 114.

Greene & Greene, The Passion and the Legacy, by Randell L. Makinson, Gibbs Smith Publisher, Pages 82, 83.

Associates

Ted Bosley's book *Greene & Greene*, Phaidon Press, 2000, reveals the following:

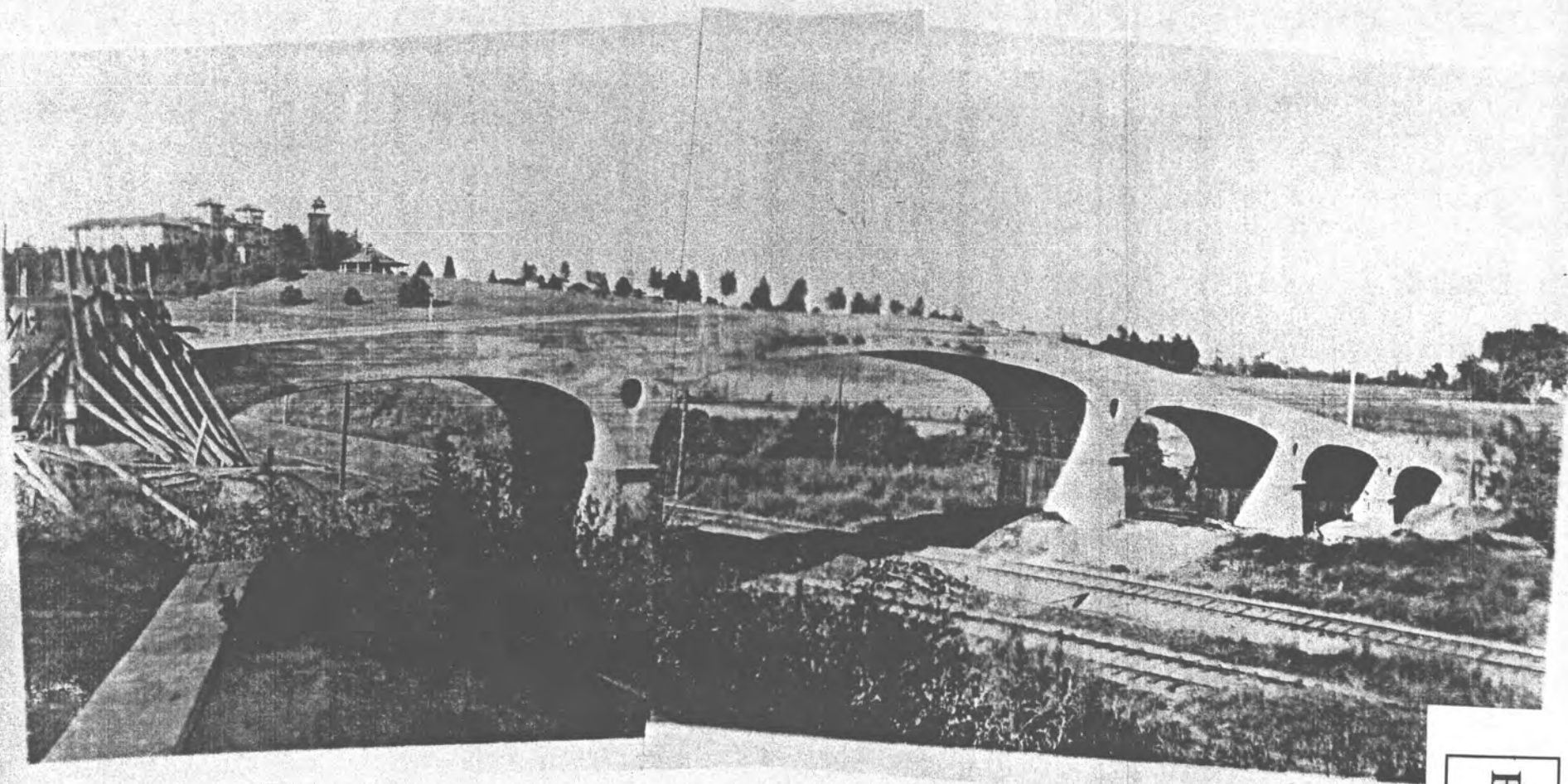
Page 91:An extra pillar, conceived by committee after the fact, was inserted into the design, clumsily bisecting the graceful arc of the main span and ruining the purity of the design. A few months later, a critical article appeared in *Architect and Engineer* suggesting that it was not the architectural design that was at fault but the engineering specifications - in particular the faulty placement of reinforcement bars - that caused the cracking. The damage to the Greenes' reputations and self-esteem, however, was done.

Telephone

626 449 4600

Facsimile

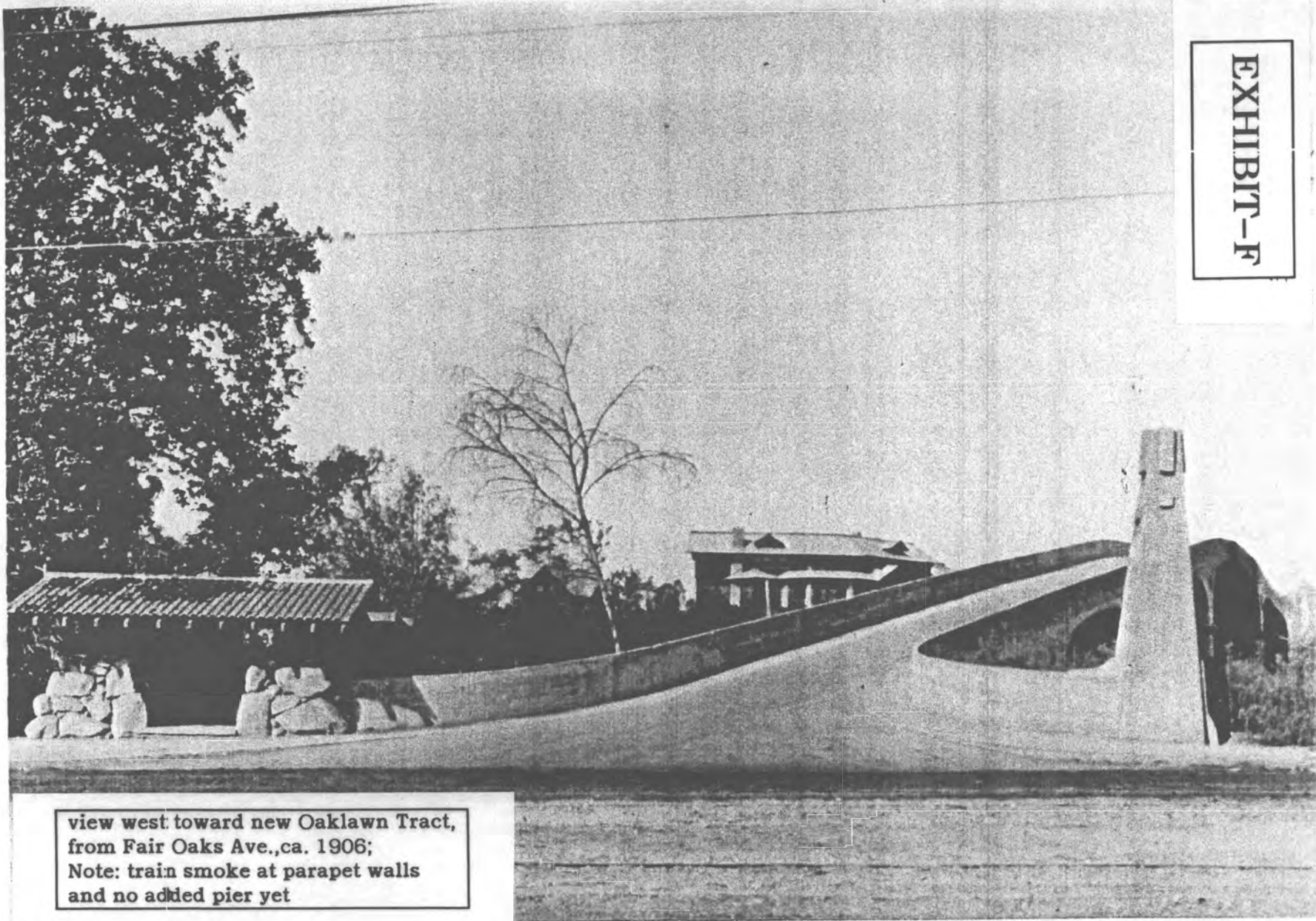
626 449 2059



view north east (toward old Raymond Hotel)
ca. 1906

EXHIBIT-E

EXHIBIT-F



view west toward new Oaklawn Tract,
from Fair Oaks Ave., ca. 1906;
Note: train smoke at parapet walls
and no added pier yet

EXHIBIT I

SIGNIFICANCE OF THE OAKLAWN TRACT

EXHIBIT-G

This tract represents one of the only subdivisions where the brothers Charles and Henry Greene had a role in the planning and design. The Greenes designed the entry portals, the perimeter fence, the bridge over the railroad tracts, and the waiting station on Fair Oaks Avenue. Although there is no direct evidence of their input into the layout of the subdivision itself, it is reasonable to assume that they were since they designed and built the bridge, fence, and portals which set the tract apart and play a major part in defining its character.

The bridge, fence, and portals are all landmarks of the City of South Pasadena. The bridge and waiting station are also listed on the National Register of Historic Places.

Development of the Oaklawn tract began in 1905. It was promoted by the South Pasadena Realty and Improvement Company, whose owner was S.W. Ferguson. Mr. Ferguson contracted with Charles and Henry Greene to design the perimeter fence, the entry portals, and the bridge to the waiting station on Fair Oaks Avenue. The special issue of the Pasadena Daily News issued for the Rose Parade on New Years Day, 1905, shows the entry to the tract with oak trees in the center of the street, pepper trees along the east side, and palms along the west side. Some of the older residents of the area remember at least one oak in the center of the street in a craftsman-style, arroyo stone well. These no longer exist. }

The January 14, 1905, issue reported that Mr. Ferguson was offering a rebate of 25% of the cost of a house built in the tract if the buyer bought between 60 and 100 feet of frontage and began construction immediately. Nonetheless, only two homes were built in 1905 -- numbers 325 and 431.

In 1907, George Lawrence Stimson, the well known architect, bought the northern half of the Oaklawn tract for \$60,000. This portion is that nominated here as an historic district. The remainder of the tract was not developed until the 1920's and 1930's, and has a distinctly different style from that of houses on Oaklawn Avenue itself.

The Pasadena Star of February 18, 1907 reported that two modern, costly residences were planned for Oaklawn Avenue, one including an office. This was 304 Oaklawn, where Mr. Stimson lived while

developing most of the rest of the tract. Records of the South Pasadena Planning Department indicate that Stimson designed at least ten of the twenty-four residences on the street. Although the ten houses vary in exterior appearance, they are all representative of the Craftsman period and of G. L. Stimson's work.

Of the twenty four houses on the street, eighteen were built prior to 1925. These are the ones which give the street its character. the remaining houses are not in the same style or from the same period, but they do not detract from the overall appearance of the area. Oaklawn is primarily a tract of "large and costly homes" in the Craftsman style. It is also one of the largest collections of the work of G.L. Stimson, and shows the variety of his designs over the first two decades of this century.

The attached list describes the individual properties and gives basic information about each.

*from
files of:*

Raymond Girvigian FAIA
1401-B Fair Oaks Box 220
So. Pasadena, CA 91031-0220
323/682-3848

Ca. 7-83



05/02/2001 15:32 6263559394

KRAKOWER

PAGE 01



krakower & associates
structural engineers

150 white oak drive
arcadia, ca 91006

626 355-6088
fax 626 355-9394

Memo

To: Ray Girvigian FAIA

From: Michael Krakower FASCE

Re: Oaklawn Bridge Pier Removal

Date: 1 May 2001



The ad hoc design review committee for the bridge has requested a brief structural description of the conditions related to the added reinforced concrete pier under the span that crosses the railroad tracks noted on our drawings as grid "1A".

The pier was added after the bridge was put into service. I have analyzed the structural capacities of the bridge using the recent field testing results for the as found materials of construction. Two original record drawings have also been reviewed. The first drawing is clearly attributed to Greene and Greene Architects and M. de Palo Consulting Engineer. There is no mention of the designer responsible for the second drawing showing the added pier and the drafting style is different from the Greene and Greene sheet.

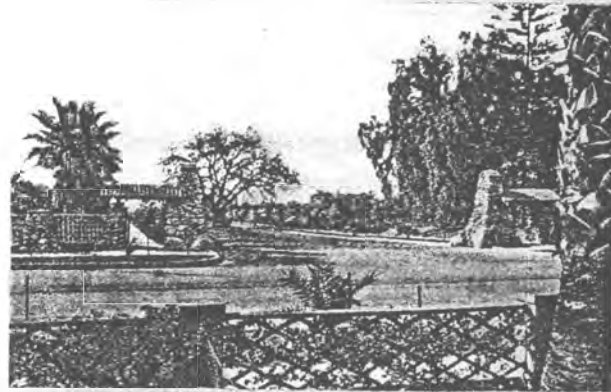
Based upon historical written records, cracks formed in the bridge deck and parapets near the original pier locations in all spans soon after the original shoring was removed. These written records attributed the crack formation to a lack of reinforcing steel at the top of the bridge deck. The cracks could also be caused in part by restrained shrinkage from the actual construction sequence.

The original designers performed load tests representing anticipated mechanized vehicle and pedestrian loads of the time. They recorded adequate strength and deflection control of the longest span over the railroad tracks without the added pier. Early design techniques with reinforced concrete assumed some hinge formation (cracks) might occur at the ends of a span and more reinforcing steel was placed at the bottom mid-span region. While we would not necessarily do that today, the bridge has stood the test of time with the cracked sections.

In my opinion, the added pier does not contribute to the reduction of the original stresses from the weight of the bridge in the span over the railroad tracks. The added pier is a non-contributing structural element to the Greene's original design.



MONG the oak and orange trees, almost at the Southern end of Pasadena's most famous boulevard, this presents a fitting climax to the beautiful row of artistic homes that line South Orange Grove Avenue.



view south Oaklawn Ave. Portals entrance (RG)

Build you a house in a garden of homes.

The pure air and sunshine of the country, the abundant trees, the sweeping lawns and flowers, offer a setting for homes, with all the city comforts



view south Oaklawn Ave. (RG)

and conveniences, which is unexcelled anywhere in the Italy of the American continent.

Within one block of the Hotel Raymond and the Pasadena electric short line to Los Angeles.

The stores and office buildings and theatres of Los Angeles

are scarcely twenty minutes distant. The churches and schools of Pasadena are within convenient reach by local car service, while to those of South Pasadena it is a pleasant walk.

Enjoy the combined advantages of the country and the city.

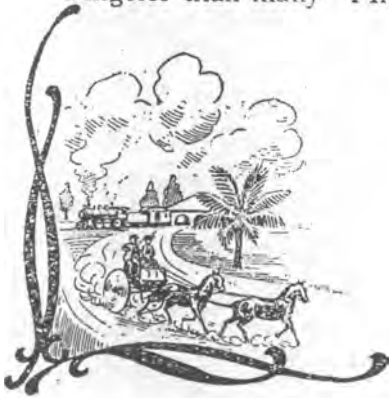


Situated on the crest of a promontory that overlooks South Pasadena, the Raymond Hotel golf links and San Gabriel valley, Oaklawn is beyond the smoke and dirt of the city, yet by the high speed "Short Line" actually nearer the heart of Los Angeles than many "High Priced"



view north Oaklawn Ave., ca. 1907-10 RG

sections within the city limits. Because of its country surroundings and lofty open situations the air is naturally much purer, and infinitely more healthful than that of the congested city. Coolness in summer and good drainage are other benefits of this high location.



3

3



view north Oaklawn toward Columbia St. RG

Here the city dweller will find the accustomed conveniences, such as electricity, gas and telephone, at city prices; both local and suburban street cars within one block, and the three Raymond railroad stations close at hand. He will

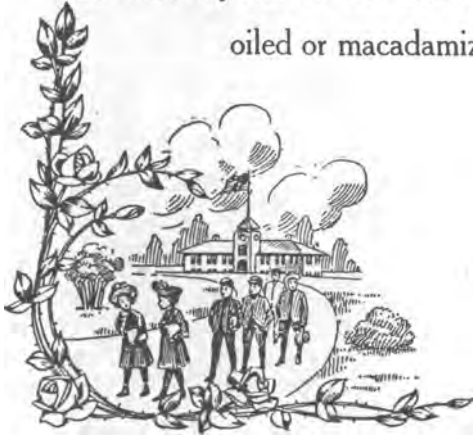
In an age that has awakened to the hygienic importance of pure water, how refreshing it is to think of the clear, cool mountain water that is piped thru Oaklawn.

Oaklawn has solved the problem of suburban residences.



also appreciate the fact that Oaklawn enjoys a suburban scale of taxes and minimum insurance rates.

In warm or wet weather the contrast between the dusty, rutty or muddy streets of the residence portion of the city and the clean smoothly oiled or macadamized



New South Oaklawn Ave. @ #423 (RG)

streets of Pasadena is sharply impressed.

Between the slow, uncomfortable city trolley and the speedy, heavy suburban car, there is, of course, no comparison. The city's noise and dust and discomforts are far enough away.

5



New Southwest @ #431 Oaklawn, Ca. >1905-07 (RG)

to the North that grand wall of the Sierras with its towering peaks, which we so often see capped with snow glistening in the bright sunlight, is too famous a view to need special mention.

On every side the verdant foliage of

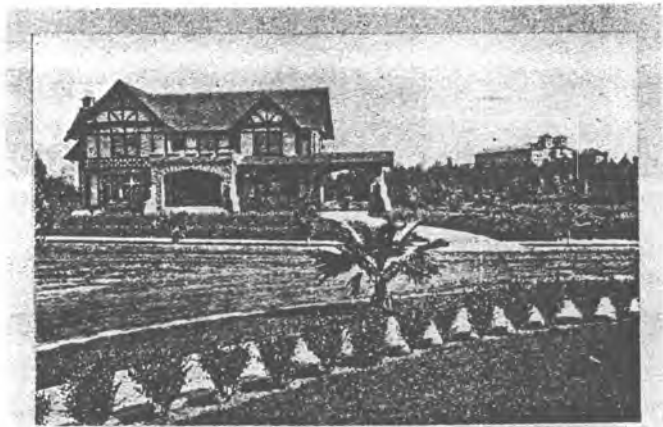
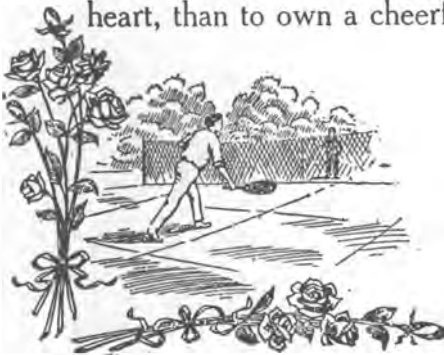
Every point in Oaklawn a view point.

Across the San Gabriel valley and the rolling country side to the far blue distance where loom the beautiful San Jacinto mountains, is one of the most fascinating pictures in Southern California;



orange, eucalyptus, pepper and other odoriferous trees, burdens the balmy air with a perfume of which the city dweller knows little.

What could be more desirable to the gentleman of taste, who has the welfare of his family most at heart, than to own a cheerful home



*view north @ Oaklawn Ave. # 412 ca. 1911?
note Raymond Hotel to right dist. bkgd. RH*

surrounded with flowers and orange trees, among refined neighborhoods in this private park. Away from the evil influences of the crowded city, his children have here plenty of room to romp and play in the pure air and sunshine of the country, with all the best advantages of schools and churches.

A good water supply, often an important item in California, is among the points in which Oaklawn is most fortunate. The north half of the tract altho actually just within the city limits of South Pasadena, is supplied by the Pasadena Land & Water Company, the same system that furnishes Orange Grove Boulevard.

In addition to the city comforts, Oaklawn offers many advantages exclusively its own. Among others might be mentioned the absence of those forests of unsightly poles and net-work of all kinds of wires that limit the view and detract so much from the front of many otherwise good properties. Telephone and electric light wires for lots on the west side of Oaklawn Place are brought in from Fremont street in the rear, and lots on the east side are connected by a line of wires brought down the back property line.



The roads thru Oaklawn are smoothly oiled and sanded, which will particularly recommend it to the owners of automobiles, as well as fine teams. To the centre of Pasadena we have the newly asphalted Fair Oaks Avenue, while from Oaklawn to Los



New Southwest Oaklawn @ #415 <1907? R4

Angeles is but a quick dash. Oaklawn joins one end of the magnificent new Huntington Boulevard, between Pasadena and the city, which is now in course of construction.

Everyone appreciates the advantages of a home



in a fine neighborhood; many know how unpleasant it is to live in an elegant residence adjoining some unsightly old house or barn. The houses built in Oaklawn thus far are uniformly elegant, tho of diversified and pleasing architecture. Oaklawn is adequately provided with building restrictions which prevent the lowering of the present high character of the neighborhood, while Columbia street, from which one enters Oaklawn, is being extensively improved, as in fact are other nearby streets.

We are now building a large, roomy house, tiled roof, stucco exterior, after the very latest school of design, and contemplate the construction of others in the near future.

The exceptional atmosphere of Oaklawn is partly due to its seven hundred and fifty feet of altitude. Los Angeles is only two hundred and eighty feet above the sea.



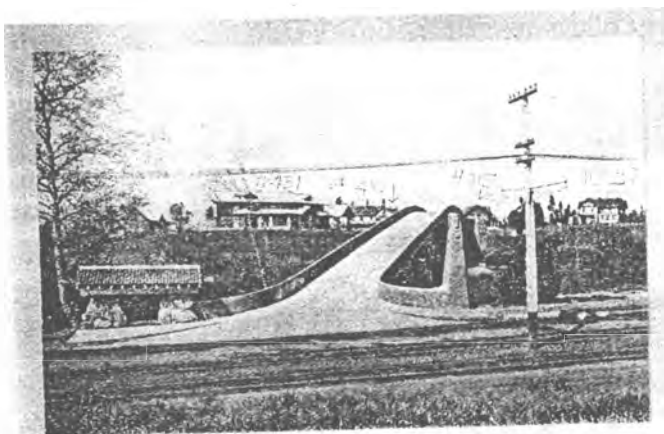
While our lots, as shown on the map (see map on last page), have from sixty to eighty-foot frontage, it is our wish to sell them in places of from one hundred to one hundred and twenty-foot frontage. All the residences that have been built so far in Oaklawn have been



View Fremont Lane north toward #423 (R5)

placed upon premises of one hundred or more feet frontage, and we feel confident that most of our customers will also want that amount.

Building restrictions prevent the construction of residences costing less than five thousand dollars. They must

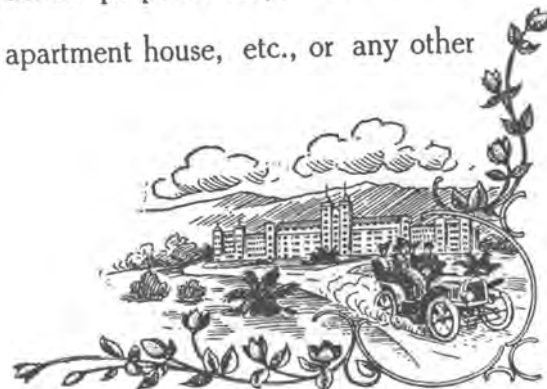


*Oaklawn Bridge & Waiting Station, now West
Note foot of bridge @ LA-Pass, interurban line @
Fair Oaks Ave. (R5)*

structure whatever, than a first-class private residence, with or without the customary outbuilding, including a private stable, shall be erected or permitted on said premises or any part thereof.

face the front line of the premises and be at least forty feet therefrom, and seven and one-half feet from the sides.

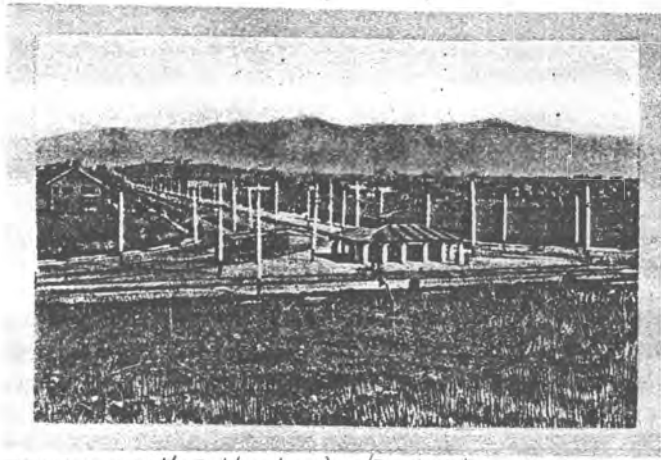
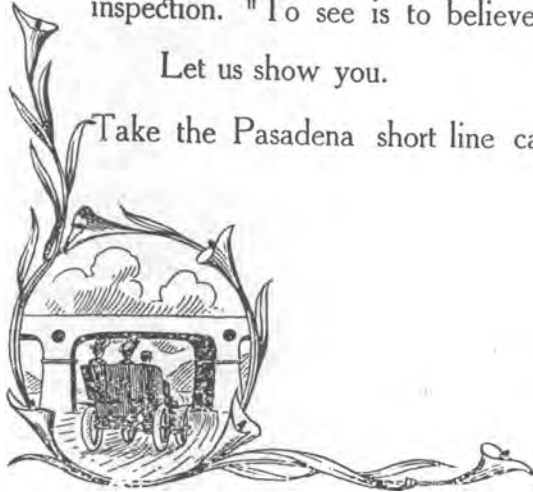
The restrictions provide that said premises shall be used for residence purposes only, and that no apartment house, etc., or any other



All we have told you, however, merely hints of the advantages of Oaklawn as a home place, and our pictures give but an inadequate idea. We would ask a personal inspection. "To see is to believe."

Let us show you.

Take the Pasadena short line car,



*new north @ Huntington (Dr.) & Oak Ave.
(R.G.)*

get off at the Hotel Raymond and walk just one block west on Columbia Street to our office and the imposing entrance of Oaklawn.

Even tho you have no intention of purchasing,

13

we shall be glad to show you thru Oaklawn Place, and answer any inquiries which your visit may occasion, without implying the least obligation to purchase.

G. LAWRENCE STIMSON

205 Los Angeles Trust Bldg.

Los Angeles

Home Phone A-8399



Portals - ca 1905



Scale 100' to 1"

FAIR OAKS AVENUE

Note: Oakburn Bridge "1,3,500(\$?) Reinf. Conc."

This map copied from
G.L. Stimson's Tract
Brochure ca. 1907-11
by permission of Linda McDonald
© 1973 Oaklawn Phase, Inc. (A.
(RG) map's earlier than Brochure

R

consultation
research
planning
restoration

Vw. S.W., @ FAIR OAKS AVE.
WAITG. STA., BRIDGE, & WWI BLDG.

Vw. West: WAIT. STA.
& OAKLAWN BRIDGE

Vw. South, WAIT. STA.
& BRIDGE, @ FAIR OAKS AVE.

grievigian photos
5/14/01, page 1

R

consultation
research
planning
restoration



View North, BRIDGE near Fair Oaks Ave.
WWI Memorial Bldg. beyond



View West, WAITING STATION
@ FAIR OAKS AVE. NUE



View North, BRIDGE entry
@ FAIR OAKS AVE.

Girvigan photos
5/14/01, page 2

R

consultation
research
planning
restoration



View S.W., BRIDGE
@ FAIR OAKS AVE NUE



View S.E. from BRIDGE
toward, WAITING ST A.



View North, under BRIDGE
STONE SEATS detail

Girvigan photos
5/14/01, page 3

R

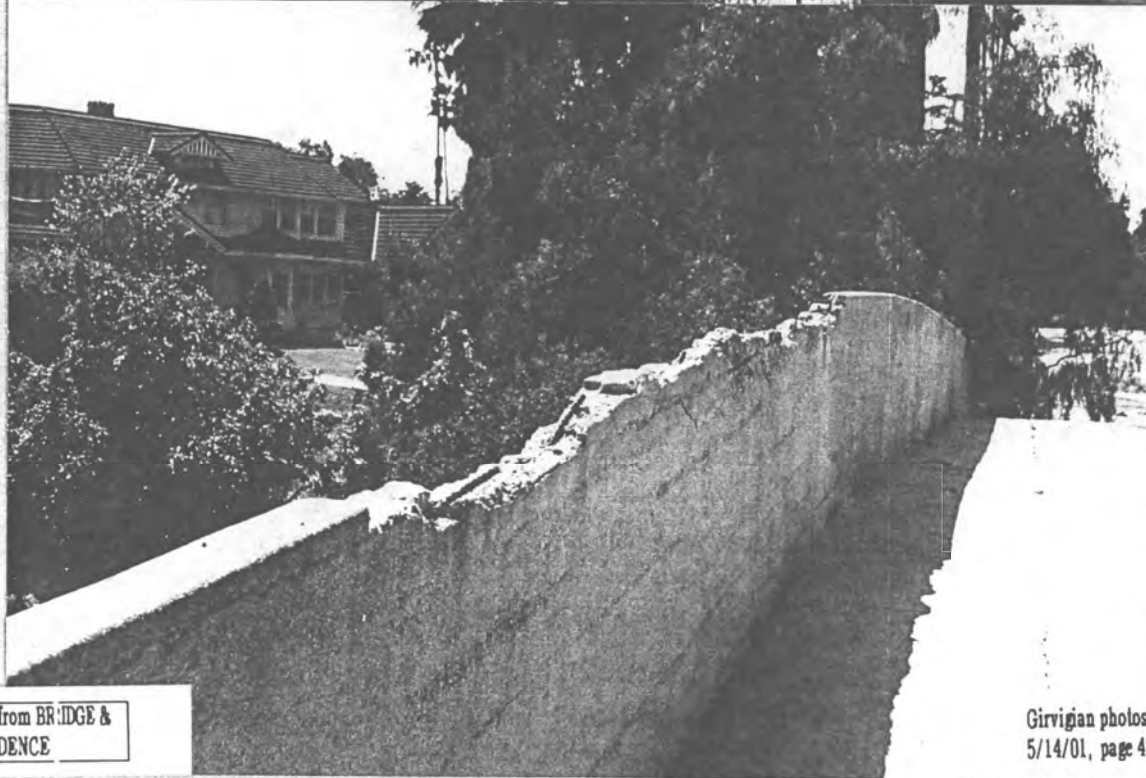
consultation
research
planning
restoration



View North of BRIDGE span #2
detail of added NO N-HISTORICAL PIER



View East under BRIDGE
detail of added NO N-HISTORICAL PIER



View S.W. from BRIDGE to
1905 RESIDENCE

Girvigan photos
5/14/01, page 4

R

consultation
research
planning
restoration

OAKLAWN AVE. ENTRY @ COLUMBIA ST.
GREENE & GREENE PORTALS flanking E & W.

OAKLAWN AVE. ENTRY @ COLUMBIA ST.
GREENE & GREENE PORTALS flanking E & W.

View North, detail of West
Portal (Oaklawn @ Columbia)

Girvigan photos
5/14/01, page 5